
SOUTH AND WEST PLANS PANEL

**Meeting to be held in Civic Hall, Leeds on
Thursday, 23rd March, 2017
at 1.30 pm**

MEMBERSHIP

Councillors

J Akhtar	J Bentley	B Anderson	R Finnigan
D Congreve		R Wood	
M Coulson			
C Gruen (Chair)			
E Nash			
A Smart			
C Towler			

**Agenda compiled by:
Andy Booth
Governance Services
Civic Hall
Tel: 0113 24 74325**

A G E N D A

Item No	Ward	Item Not Open		Page No
1			<p>APPEALS AGAINST REFUSAL OF INSPECTION OF DOCUMENTS</p> <p>To consider any appeals in accordance with Procedure Rule 15.2 of the Access to Information Rules (in the event of an Appeal the press and public will be excluded)</p> <p>(*In accordance with Procedure Rule 15.2, written notice of an appeal must be received by the Head of Governance Services at least 24 hours before the meeting)</p>	
2			<p>EXEMPT INFORMATION - POSSIBLE EXCLUSION OF THE PRESS AND PUBLIC</p> <p>1 To highlight reports or appendices which officers have identified as containing exempt information, and where officers consider that the public interest in maintaining the exemption outweighs the public interest in disclosing the information, for the reasons outlined in the report.</p> <p>2 To consider whether or not to accept the officers recommendation in respect of the above information.</p> <p>3 If so, to formally pass the following resolution:-</p> <p>RESOLVED – That the press and public be excluded from the meeting during consideration of the following parts of the agenda designated as containing exempt information on the grounds that it is likely, in view of the nature of the business to be transacted or the nature of the proceedings, that if members of the press and public were present there would be disclosure to them of exempt information, as follows:-</p> <p>No exempt items or information have been identified on the agenda</p>	

Item No	Ward	Item Not Open		Page No
3			LATE ITEMS To identify items which have been admitted to the agenda by the Chair for consideration (The special circumstances shall be specified in the minutes)	
4			DECLARATIONS OF DISCLOSABLE PECUNIARY INTERESTS To disclose or draw attention to any disclosable pecuniary interests for the purposes of Section 31 of the Localism Act 2011 and paragraphs 13-16 of the Members' Code of Conduct.	
5			APOLOGIES FOR ABSENCE	
6			MINUTES - 16 FEBRUARY 2017 To approve a s a correct record, the minutes of the meeting held on 16 February 2017	3 - 8
7	Farnley and Wortley		APPLICATION 16/05912/OT - LAND AT WHITEHALL ROAD, NEW FARNLEY To receive and consider the attached report of the Chief Planning Officer regarding an outline planning application (all matters reserved except for means of access to, but not within, the site) for circa 130 dwellings to include the demolition of 632 and 634 Whitehall Road.	9 - 42
8	Adel and Wharfedale		APPLICATION 16/06222/OT - LAND TO THE EAST OF OTLEY ROAD, ADEL, LS16 8FE To receive and consider the attached report of the Chief Planning Officer regarding an outline application for residential development (use Class C3) for up to 100 dwellings and land reserved for primary school with construction of vehicular access from Otley Road to the north west and Ash Road to the south, areas of open space, landscaping, ecology treatments and associated works.	43 - 68

Item No	Ward	Item Not Open		Page No
9	Horsforth		<p>APPLICATION 16/07825/FU - 11 CHURCH CRESCENT, HORSFORTH</p> <p>To receive and consider the attached report of the Chief Planning Officer regarding an amendment to application 16/07825/FU – single storey side extension from flat roof to pitched</p> <p><u>Third Party Recording</u></p> <p>Recording of this meeting is allowed to enable those not present to see or hear the proceedings either as they take place (or later) and to enable the reporting of those proceedings. A copy of the recording protocol is available from the contacts named on the front of this agenda.</p> <p>Use of Recordings by Third Parties– code of practice</p> <ul style="list-style-type: none"> a) Any published recording should be accompanied by a statement of when and where the recording was made, the context of the discussion that took place, and a clear identification of the main speakers and their role or title. b) Those making recordings must not edit the recording in a way that could lead to misinterpretation or misrepresentation of the proceedings or comments made by attendees. In particular there should be no internal editing of published extracts; recordings may start at any point and end at any point but the material between those points must be complete. 	69 - 76

To all Members of South and West
Plans Panel

Planning Services

The Leonardo Building
2 Rossington Street
Leeds
LS2 8HD

Contact: David Newbury
Tel: 0113 378 7990
david.m.newbury@leeds.gov.uk

Our reference: SW Site Visits
Date: March 2017

Dear Councillor

SITE VISITS – SOUTH AND WEST PLANS PANEL – THURSDAY 23 March 2017

Prior to the meeting of the above South and West Plans Panel the following site visits will take place:

Time	
10.10am	Depart Civic Hall
10.25am	16/05912/OT – Land at Whitehall Road, New Farnley
11.00am	16/07825/FU - 11 Church Crescent, Horsforth, LS18 5LF
11.25am	16/06222/OT – Land to the East of Otley Road, Adel, LS16 8FE
12.00 noon	Return to the Civic Hall

For those Members requiring transport, a minibus will leave the Civic Hall at 10.10am. Please notify David Newbury (Tel: 0113 378 7990) if you wish to take advantage of this and meet in the Ante Chamber at 10.05am.

Yours sincerely

David Newbury
Group Manager

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SOUTH AND WEST PLANS PANEL

THURSDAY, 16TH FEBRUARY, 2017

PRESENT: Councillor C Gruen in the Chair

Councillors J Akhtar, B Anderson,
J Bentley, D Congreve, M Coulson,
T Leadley, E Nash, A Smart, C Towler and
R Wood

On the morning of the Panel Members visited the sites at 14 Stubley Farm Mews, 7 Ring Road, Beeston and Middleton Methodist Church. These visits were attended by Councillors C Gruen, D Congreve, E Nash, C Towler, B Anderson, R Wood and T Leadley.

62 Declarations of Disclosable Pecuniary Interests

There were no declarations of disclosable pecuniary interests. Councillor D Congreve informed the Panel that he had previously commented on Application 8 – Application 16/06842/FU – 7 Ring Road, Beeston Park, Middleton, Leeds, LS11 5LG and to avoid concerns being raised in respect of pre-determination he would take no part in the discussion or voting on this application.

63 Apologies for Absence

Apologies for absence were submitted on behalf of Councillor R Finnigan.

Councillor T Leadley was in attendance as substitute.

64 Minutes - 19 January 2017

RESOLVED – That the minutes of the meeting held on 19 January 2017 be confirmed as a correct record.

65 Application 16/01480/FU - Middleton Methodist Church, Hopewell View, Middleton, LS10 3TE

The report of the Chief Planning Officer presented an application for the change of use of a former church and hall to form 9 flats, including external alterations and partial demolition of ancillary buildings and on-site parking.

There was a site visit prior to the meeting and site plans and photographs were displayed and referred to throughout the discussion of the application.

Further issues highlighted in relation to the application included the following:

- The site fell within a densely populated residential area.
- Relation of the site to nearby properties was highlighted.

Draft minutes to be approved at the meeting
to be held on Thursday, 23rd March, 2017

- Parking and access arrangements were explained.
- Objections had included overlooking of existing properties and potential noise disturbance. Obscure glazing would be used where there was a possibility of overlooking.
- Initial plans had proposed fifteen flats. This had since been revised down to nine.
- There would be some demolition work and also minor external alterations to the remaining building which included access areas and the inclusion of roof lights.
- A parking area was to be provided at the rear.
- There had been alterations to internal layouts to ensure no living areas would overlook existing properties.
- There had been a late objection with a request for a higher wall to the rear to protect privacy of existing residents.
- The application was recommended for approval.

A local resident addressed the Panel with concerns and objections to the application. These included the following:

- Main problem and concern was with multiple occupancy that the flats would create and disturbance due to the proximity to existing houses and gardens.
- Alternative entrances to the site were suggested to reduce noise disturbance.
- There would be additional problems with increased traffic and parking.
- Local Ward Councillors and The Local MP had been sympathetic to residents' concerns.
- The access and exit if the parking area was potentially dangerous.
- It was felt that concerns of local residents had not been listened to.
- If the plans were to go ahead it was requested that there be conditions regarding construction times and prevention of light pollution.

The applicant's representative addressed the Panel. Issues highlighted included the following:

- The proposals would bring a non-designated heritage asset back into use. There had been no other interest in the building other than for housing.
- It was not felt that noise made from resident's access would be sufficient to cause disturbance to existing residents. There would not be as much sound disturbance as when the building was used as a church.
- Even though there had not been an objection from highways, extra land had been made available for more car parking. Visibility from the access point was sufficient.
- The applicant had been mindful of objections and had worked closely with planning officers to address resident's concerns.

In response to comments and questions, the following was discussed:

- The corridor access was 5 metres from the nearest property, noise levels this would cause were considered to be acceptable.
- The possibility of using noise insulating materials on the access corridor.
- There would be a condition for boundary treatments.
- Height of the barrier wall.

RESOLVED - That the application be granted as per the officer recommendation and the conditions outlined in the report and further discussion regarding boundary treatments and height of the boundary wall (condition 11)16.

66 Application 16/06842/FU - 7 Ring Road, Beeston Park, Middleton, Leeds, LS11 5LG

The report of the Chief Planning Officer presented an application for the change of use of a tattoo parlour to hot food take away (A5 use) and alterations involving addition of chimney encased extract flue to side of 7 Ring Road, Beeston Park, Beeston, LS11 5LG.

There was a site visit prior to the meeting and site plans and photographs were displayed and referred to throughout the discussion of the application.

Further issues highlighted in relation to the application included the following:

- External alterations included the inclusion of a chimney encased flue to the side of the building.
- The application was for the ground floor of the premises that had remained vacant for a long while.
- There would be minor internal alterations.
- There was forecourt parking to the front and storage to the rear of the building.
- Objections had been submitted by all 3 Local Ward Councillors due to the high proliferation of takeaways in the area.
- The siting of the fan in the extract flue was above the roof line and would counteract problems with fumes.
- The application had previously been refused due to the proposed length of opening hours and these had now been reduced to 1000 to 2200.
- The application was recommended for approval. Conditions to include litter management; hours for opening and deliveries; full details of the extraction system to be supplied.

In response to Members comments and questions, the following was discussed:

- Concern regarding rubbish to the rear of the property and the potential for rodents. It was asked whether there could be a condition to have

this cleared. It was reported that there was a condition for waste storage and standards to be met for waste food collection. Tidying of the area would be an Environmental Health matter and they would need to investigate and take enforcement action where necessary.

- Policy on Hot Food Takeaways – the Panel was informed that there was due to be a report to Development Plans Panel.
- Whether building control could be informed of the poor condition of the garage doors.

RESOLVED - That the application be granted as per the officer recommendation and the conditions outlined in the report. Condition of the site to be referred to Environmental Health and Building Control in terms of tidying the site and to be reported back to the Chair.

67 Application 16/07926/FU - 14 Stubley Farm Mews, Morley, Leeds, LS27 9ND

The report of the Chief Planning Officer presented an application for the conversion of an integral garage to a habitable room at 14 Stubley Farm Mews, Morley, LS27 9ND.

There was a site visit prior to the meeting and site plans and photographs were displayed and referred to throughout the discussion of the application.

Further issues highlighted in relation to the application included the following:

- The property was 3 storeys with dormers, an internal garage and occupied a corner plot.
- The applicant had created access to the rear of the property for car parking under permitted development rights.
- An application had previously been refused as the loss of the garage would reduce parking provision. A visit to the site had determined the garage to be sub-standard under new guidance and not fit for the purpose of parking a family vehicle.
- Parking provision had been addressed with the provision of the space to the rear of the property and there had not been any concern expressed by Highways.
- Objections had been received from neighbours regarding pedestrian safety due to the parking at the rear of the property.
- The application was recommended for approval with a condition that the space to the rear of the property is retained for parking.

A neighbouring resident addressed the Panel with concerns and objections to the application. These included the following:

- Noise and fumes to neighbouring property – when reversing into the drive, the car's exhaust came within 4 feet of the kitchen window.
- When the neighbouring property was bought it was not anticipated that the benefit of the garden to a young family would be lost.

- Children's bedroom windows could not be left open due to fumes and there was also disturbance due to the applicant working shifts.
- There were narrow paths round the street and the concealed driveway put children at risk while they were out playing.
- The removal of the wall to create the driveway was a breach of planning legislation.
- The garage was large enough to park a vehicle and there were also 6 off road spaces on the site that could be used.

The applicant addressed the Panel. Issues highlighted included the following:

- An application had been submitted in March 2016 but was withdrawn as Highways had stated that access to make the parking at the rear was not wide enough. Following further advice, work was undertaken to widen the access to create a space at the rear. This was done at a cost of in excess of £7.5k and was done as a permitted development. A further application was submitted in October 2016 and refused. Further information came to light in December 2016 which showed that the garage was sub-standard under street design guidance and that therefore there was no loss of a parking space. The applicant was then invited to submit a further application.
- The proposal was to convert the garage to create more living space. There would not be any external alterations.
- It had not been mentioned prior to the application that there would be a condition to retain the rear parking space.
- It considered that the condition to retain the parking space failed key tests under government guidelines and it was requested that this condition be removed.
- If it had been noted at the first application that the garage was sub-standard then there would have been no need to create the extra space at the rear. There had been a series of errors and the application should have just been a non-material change to an integral garage.

In response to Members comments and questions, the following was discussed:

- With regard to the legality of the condition to retain the additional parking space, members were informed that consideration needed to be given to how many parking spaces were required. At the original planning permission for the property there was a requirement for 2 spaces and this condition ensured that was retained.
- Comments were made that garage conversions on these kind of properties were common and that although the property was sited in an awkward position, the applicant had found a way to create additional parking. It was also felt that the condition to retain the rear parking space was reasonable.

RESOLVED - That the application be granted as per the officer recommendation and the conditions outlined in the report.

68 Date and Time of Next Meeting

Thursday, 24 March 2017 at 1.30 p.m.



Originator: Kate Mansell

Tel: 0113 378 8019

Report of the Chief Planning Officer

SOUTH AND WEST PLANS PANEL

Date: 23rd March 2017

Subject: Planning Application 16/05912/OT - Outline planning application (all matters reserved except for means of access to, but not within, the site) for up to 130 dwellings to include the demolition of 632 and 634 Whitehall Road on land at Whitehall Road, New Farnley, Leeds.

APPLICANT

Redrow Homes Limited and
Park Lane Homes Limited.

DATE VALID

21st September 2016

TARGET DATE

31st March 2017 (PPA)

Electoral Wards Affected:

Farnley and Wortley

☐ Yes

Ward Members consulted
(referred to in report)

Specific Implications For:

Equality and Diversity ☐

Community Cohesion ☐

Narrowing the Gap ☐

RECOMMENDATION: DEFER and DELEGATE to the Chief Planning Officer for approval subject to conditions to cover those matters outlined below (and any others which he might consider appropriate) and the completion of a S106 agreement to secure the following:

- i. Affordable Housing – 15% (with a 60% social rent and 40% submarket split);
- ii. A contribution of £30,000 towards the creation of a 20mph speed limit on the neighbouring highways;
- iii. Public open space on site of the size to comply with Core Strategy Policy G4;
- iv. Provision of a Sustainable Travel Fund of £62,562.50;
- v. Travel Plan Review fee of £2,650.

In the circumstances where the S106 has not been completed within 3 months of the resolution to grant planning permission, the final determination of the application shall be delegated to the Chief Planning Officer

1. Time limit for application for approval of Reserved Matters and commencement.
2. Approval of outstanding details following outline permission.
3. Plans to be approved.
4. Reserved Matters in accordance with the Masterplan to a maximum of 130 dwellings.
5. Samples of walling, roofing and surfacing material to be approved.
6. Existing and proposed levels.
7. Details of means of enclosure.
8. Details of bin stores.
9. Retention of existing hedgerows and trees and any removal to be agreed.
10. Tree protection measures
11. Landscape scheme.
12. Implementation of landscape scheme.
13. Landscape management plan.
14. Biodiversity enhancement condition.
15. Method statement for the control of Japanese Knotweed.
16. Details of drainage infrastructure and balancing pond.
17. Feasibility study into the use of infiltration drainage methods.
18. Details of surface water drainage.
19. Method statement for interim drainage measures.
20. Travel Plan.
21. Approved Vehicular Access.
22. Specified Off-site Highway Works.
23. Cycle provision.
24. Footpath connections.
25. Statement of construction practice
26. Vehicle spaces to be laid out prior to development being occupied.
27. Maximum gradient to access
28. Maximum gradient to driveways
29. Provision of visibility splays of 2.4 metres x 120 metres at the Whitehall Road junction
30. Highway condition survey
31. Contamination reports and remedial works.
32. Unexpected contamination.
33. Verification reports.
34. Soil importation condition
35. Details to achieve 10% of energy needs from low carbon energy.
36. Electric vehicle provision.
37. Scheme of intrusive site investigations for the shallow coal workings.

1.0 INTRODUCTION

- 1.1 This outline planning application is presented to Plans Panel on the basis that it represents a major development, which the Chair, in conjunction with the Chief Planning Officer considers is controversial. The site is identified as a Protected Area of Search (PAS) within the Saved Policies of the Adopted Unitary Development Plan (UDP) and it is retained as a PAS site within the Pre-Submission Draft Site Allocations Plan (SAP).

2.0 PROPOSAL

- 2.1 This application seeks outline planning permission for the residential development of a 5.28 hectare site to deliver up to 130 dwellings, which could provide a combination of 1, 2, 3 and 4 bedroom units comprising a mixture of terraced, semi-detached and detached dwellings. The outline application seeks to consider the principle of development and the means of access into the site only. Matters of site layout, the

appearance of the dwellings, the scale of development and the landscaping of the site (the Reserved Matters) are reserved for future consideration and accordingly, such matters do not form part of the assessment of this application.

- 2.2 The application is supported by a Design and Access Statement and an illustrative plan, which indicates that the site can accommodate circa 130 dwellings (maximum). The application also includes a Transport Assessment, which has been undertaken on the basis of 130 dwellings and this maximum figure therefore forms the basis for the assessment of this proposal.
- 2.3 Means of access is defined within the Town & Country Planning (General Development Procedure) Order 2015 to cover accessibility for all routes to and within the site, as well as the way they link up to other roads and pathways outside the site. This application only relates however to the access to and not within the site.
- 2.4 In this case, a single vehicular access to the site is proposed from Whitehall Road via a new priority junction positioned between Nos.630 and 636 Whitehall Road. The access will have a minimum 5.5-metre width with 2 metre footpaths to each side, the creation of which requires the demolition of Nos. 632 and 634 Whitehall Road. The application includes an illustrative Masterplan, which also indicates a potential pedestrian link from the site onto Low Moor Side Lane between 19a and 19b Low Moor Side Lane as well as utilizing the existing pedestrian route onto Castle Ing Gardens, between Nos. 40 and 42.
- 2.5 Members are advised to note that the proposed new vehicular access into the site crosses an existing service road that runs to the front of properties at 658-594 Whitehall Road East. There are four existing access points onto Whitehall Road from this service road for these existing dwellings and there is also a grass verge with street lighting separating the service road from Whitehall Road East itself. There is presently some uncertainty regarding the status of this land; the Council's records do not show this service road as falling within the limits of adopted highway although the applicant has provided a register of title, which suggests that the land was dedicated as highway under deeds dating back to 1933. However, this uncertainty does not preclude the determination of this application. To reflect the lack of certainty about the status of this land, the applicant has completed Certificate C of the planning application form, which is for use with Certificate B (where the applicant does not own the whole site and notice is served on other owners) but where not all the owners are known (as per the service road). Accordingly, the applicant has confirmed that an advert was placed in the Yorkshire Evening Post on 15th September 2016. Any subsequent issue in relation to land ownership is a civil matter for the applicant to resolve and not a matter that can halt the determination of a planning application where the appropriate notices have been served.
- 2.6 All other details pertaining to the Reserved Matters of layout, scale, appearance and landscaping are for indicative purposes only such that they will be considered in detail at Reserved Matters stage.
- 2.7 The illustrative Masterplan indicates that the residential development will be constructed around the access road that comprises a series of cul-de-sacs rather than a looped access around the site. The site is also the subject of a blanket Tree Preservation Order, which will impact upon the site layout. In this regard, an illustrative Parameters Plan indicates the retention of a T-shaped band of trees that runs east-west across the site parallel with 19a Low Moor Side Lane to the east and

66 Castle Ings Garden to the west and then extending southwards as well as the trees along the boundary of the site to Low Moor Side Lane, to the rear of some of the properties on Whitehall Road and to the rear of 26-32 Castle Ings Gardens. An area of public open space is indicated to the rear of 26-40 Castle Ings Gardens. The Design and Access Statement advises that the gross site area is 5.28 hectares of which 4.65 hectares will comprise residential development, 0.57 hectares of open space and 0.06 hectares of incidental land. Based upon 130 dwellings, this would result in a density of circa 25 units per hectare.

- 2.8 The appearance of the houses will be determined at Reserved Matters stage although the submitted Design and Access Statement identifies that design influences will be informed by a contextual analysis of the area.
- 2.9 The scale of development is also a matter to be determined at Reserved Matters stage although the submitted Design and Access Statement identifies the houses to be 2-storey. An indicative site section has also been submitted to indicate the relationship to the existing dwellings as the site slopes from Low Moor Side Lane to Castle Ings Gardens.
- 2.10 The landscaping of the site will also be determined at Reserved Matters stage. It is noted, however, that the Design and Access Statement confirms that existing landscaping will be retained and integrated within the site with a central green swathe crossing the centre of the site to provide pedestrian connectivity as well as a new area of public open space to the north of the site.
- 2.11 To support their submission, in addition to an illustrative Masterplan, Parameters Plan and Section (which are all for illustrative purposes only and would not become approved plans) the application also includes a Design and Access Statement, Planning Statement, Statement of Community Involvement, a Landscape and Visual Assessment, a Transport Assessment and Travel Plan, a Flood Risk Assessment and a Preliminary Ecological Appraisal, an Archaeology Survey and an Arboricultural Survey.

3.0 SITE AND SURROUNDINGS

- 3.1 The application site, which presently comprises open fields, extends to 5.28 hectares in New Farnley, approximately 4 miles to the south-east of Leeds City Centre. It is broadly triangular in shape and it is entirely enclosed on two sides by existing housing. The southern boundary of the site runs to the rear of dwellings at 2-20 Walsh Lane, including land between 10 and 10b Marsh Lane and then continues to the rear of 658 to 632 Whitehall Road with 632 and 634 identified for demolition to accommodate the site access. It is noted that No.12 Walsh Lane (Plane Tree Farmhouse) is a Grade II Listed late Eighteenth Century building. The eastern boundary runs to the rear of 12 to 74 Castle Ings Gardens. The western boundary adjoins Low Moor Side Lane; approximately one third of this boundary is situated to the rear of 19-29 Low Moor Side Lane with the remainder of the boundary set back from Low Moor Side Lane by a wide grass verge with hedging and planting along the edge of the field. The opposite side of the site is mainly open fields. Finally, it is noted that there are significant level changes across the site from west to east with Low Moor Side Lane at least 10 metres higher than the level of Castle Ings Gardens.
- 3.2 The site is the subject of an Area Tree Preservation Order, which protects all trees located within the area defined by the TPO and applies to all the trees (with a stem

diameter of 75mm or more, measured at 1.5 metres above ground level) that were in existence when the TPO was made. The submitted Arboricultural Survey confirms that the site is comprised of rough grassland, which is divided by four mature, unmanaged hedgerows. In terms of trees, the majority of surveyed trees are located beyond the limits of the site, with crown spreads and root protection areas that overhang the site boundary. There are two oak trees located off site, within the rear gardens of properties adjacent to the southern site boundary, of which one is identified a notable mature oak tree of high value and quality. There are five further tree groups located within the site along the western boundary, and are all subject to a TPO. The survey considers that these groups are mostly self seeded scrub, comprising of hawthorn, willow, apple, elder and privet. Beyond the western site boundary are four further trees and two groups of trees that are all subject to a TPO and these mostly comprise semi mature sycamores. In the north of the site, close to the boundary, is a further group comprising mostly willow and hawthorn, which are subject to a TPO. Finally, there is a single rowan tree on Whitehall Road and two groups of trees on Walsh Lane comprising cypress and willow and sycamore.

- 3.3 A definitive footpath (No.147) presently runs along part of the eastern boundary of the site, accessed from a footway between 40 and 42 Castle Ings Gardens and then between 632 and 634 Whitehall Road.
- 3.4. The character of the surrounding area is essentially residential comprising circa 1930s ribbon development along Whitehall Road, principally in the form of two-storey red brick and render semi-detached dwellings and a distinctive post-War housing estate at Castle Ings Gardens, which are largely red brick bungalows. The character of properties on Walsh Lane is more bespoke, comprising detached dwellings of varying styles set within guide generous plots. The dwellings on Low Moor Side Lane also vary in form between post-War red brick semis to more recent detached stone dwellings. Due to the narrowness of the lane and the open fields to the west, Low Moor Side Lane has a more rural character. The settlement of New Farnley, which includes these existing dwellings, lies principally to the east of the application site with the land to the west falling within the Green Belt and serving the purpose of separating Leeds and Bradford.

4.0 RELEVANT PLANNING HISTORY

- 4.1 There is no planning history directly relevant to the consideration of this application.

5.0 HISTORY OF NEGOTIATIONS

- 5.1 The applicant did seek to engage in some pre-application discussions in 2014 (PREAPP/14/01017) to discuss the matter of highway and design approach only.

6.0 PUBLIC/LOCAL RESPONSE

- 6.1 The application was initially advertised by means of site notices and a press notice in the Yorkshire Evening Post, published on 14th October 2016. Following a minor modification to the description to make it clear that the access road would require the demolition of 632 and 634 Whitehall Road a further consultation exercise of objectors was undertaken.
- 6.2 At the time of writing this report, in total, 194 objection letters have been received in total with some residents having responded to both consultations. In addition, two

petitions have been received with a total of 300 signatures. A summary of the issues raised by the objection letters and petitions is set out below:

- The field was supposed to be protected until 2028 as a PAS site – it is designated as PAS land in the Publication Draft Housing Allocations Document;
- The New Farnley Vision Group have worked for years to put together the Village Design Statement, which includes the current Site Allocations Plan;
- The New Farnley Village Design Statement has now been accepted as SPD by Leeds City Council and therefore has to be taken into account. It states that the development of this site should only be considered for development as part of the new Site Allocations Development Plan and following consultation with the local community. This application would contradict the Design Statement.
- This land does not form part of the Council's housing strategy;
- There are more suitable plots of brownfield land to build upon and greenfield sites should be preserved;
- Residents purchased their houses in good faith that the land at the back of their houses would be protected until 2028 as PAS land;
- The plans do not take into account the fact that [we] (the residents) own a small portion of land, which has been transferred into their names.
- Insufficient community and service amenities in the area
- The Doctor's Surgery in the village has recently closed and is now on the Ring Road;
- Very few amenities; no GP surgery and only one shop with oversubscribed local schools;
- No Doctor/chemist or dental practices in the village;
- Traffic and congestion – it is already a problem at peak times.
- The service road fronting properties 590-658 Whitehall Road is not maintained as a public highway, nor are the two spur roads, which access Whitehall Road. The service road is a private road, owned and maintained (through insurances) by the residents and the developer does not have any rights over this road.
- Dangerous access
- Walsh Lane is very narrow and it could be used as a rat-run with increased traffic;
- Whitehall Road cannot cope with the volume of traffic and Ringways roundabout is already gridlocked every work/school day;
- The access will cross the residents' right of way and Whitehall Road is already congested;

- Roads around the site are littered with potholes and in a poor state of repair, which will be worsened by this development.
- There are at least 20 equestrian establishments between the Ring Road and Bradford and they need to use country lanes; this will increase traffic and be a risk to horses and riders;
- Bus services are inadequate;
- The highways review does not give full account of the impact of the development on the local road as it misses the key impact it will have on the resident's right of way on Whitehall Road, which will be curtailed by the access route;
- The access road is directly opposite an access to the local farm and other private dwellings, which will cause congestion;
- Too many houses being built on green fields;
- New Farnley should be kept as a village;
- Residents enjoy living in a semi-rural location and do not want additional housing, particularly as motorists drive around the village to avoid stationary traffic;
- The field contains a natural spring and is waterlogged at the back of Castle Ings Gardens where the greenspace is proposed;
- Impact on flooding;
- There is an abundance of wildlife on the site including protected species (bats);
- Impact on daylight and outlook from the bungalows on Castle Ings Gardens.
- The site is not needed to fulfil housing need in the area in the short/medium term;
- The land is high risk as it contains mine shafts
- The residents query whether or not there are any laws to stop the developer knocking down two houses to gain access, otherwise it is land locked.
- The application needs to be the subject of a full EIA as it exceeds the 5ha indicative threshold;
- The applicant did not engage in pre-application consultation, which is a breach of the NPPF and the Leeds Statement of Community Involvement.
- A resident suggests that central government ask that local councils consider brownfield development over Greenfield;
- This application enhances encroachment and the Council should be safeguarding such land as custodians of Green Belt land

- The plans do not consider the type of houses missing locally and there is no attempt to provide any communal or open space - where are the detached bungalows, playground?

6.3 Ward Members have been consulted on the application. Councillor Ann Blackburn has objected to the application on the following grounds:

- a. The site is PAS land in the UDP and is safeguarded until 2018 in the Draft Site Allocations Plan, thereby it should not be considered for development until then.
- b. Having traffic from circa 130 dwellings would substantially increase the amount of traffic on the already busy Whitehall Road. Councillor Blackburn notes that the developers have done a desk top exercise that relates to what the amount of peak traffic was up to September 2015, but obviously the traffic has increased since then. The vehicles coming/going from the proposed vehicular access on Whitehall Road would cause traffic problems for the neighbouring residents who use the side road for vehicular entry to their houses. Any increase in traffic would also affect the nearby Walsh Lane and Low Moor Side Lane, which are very narrow country roads.
- c. Councillor Blackburn is against any pedestrian links into the site as proposed on Low Moor Side Lane. It would be dangerous to have a vehicular access from Low Moor Side Lane as this is a narrow country road, which regularly has horses travelling on it from the various riding schools on Low Moor Side Lane. Some of the residents on Castle Ings Gardens have an entry from their gardens onto the Public Right of Way 149, which currently runs at the side of the proposed vehicular entry of Whitehall Road, which she would see as a safety hazard.
- d. Councillor Blackburn is also concerned that in the very recent plans, the developer wants to take down some of the trees on site, and she objects to this as these are a haven for the birds and wildlife.

7.0 CONSULTATION RESPONSES

Statutory:

- 7.1. Coal Authority: The Coal Authority concurs with the recommendations of the Preliminary Geo-environmental Investigation Report, August 2016, prepared by Lithos Consulting Limited; that coal mining legacy potentially poses a risk to the proposed development and that further intrusive site investigation works should be undertaken prior to development in order to establish the exact situation regarding coal mining legacy issues on the site. The Coal Authority recommends that the LPA impose a Planning Condition should planning permission be granted for the proposed development requiring these site investigation works prior to commencement of development. In the event that the site investigations confirm that remedial works would be needed to treat the recorded mine entries beneath any parts of the site where built development is proposed, this should be conditioned to ensure that the site layout is amended to avoid them. The condition should also ensure that any remedial works identified by the site investigation to consolidate any shallow mine workings are undertaken prior to commencement of the development.

- 7.2 Highways: The means of access into the site has been amended in the course of the application and further information supplied in relation to the Transport Assessment, which is detailed in the report below. Overall, however, the Council's Highways Officer concludes that the proposal is acceptable, subject to the requirements for a planning obligation, which will be secured by a Section 106 Legal Agreement to secure a contribution of £30,000 towards the introduction of a 20mph speed limit on neighbouring roads. It is considered that the proposed development is located in a sufficiently accessible location and it will provide a safe and secure access for pedestrians, cyclists and people with impaired mobility with appropriate parking provision such that the means of access is acceptable. Subject to the contributions in relation to off-site highway works, the development is also not considered to result in a severe residual cumulative highway impact to warrant a refusal such that it must be concluded that the proposed means of access is acceptable and the development is in accordance with Policy T2 of the Core Strategy and guidance within the NPPF.

Non-Statutory:

- 7.3 Landscape: Due to topography, existing perimeter trees and the bounding of much of the site by existing houses, visual impact issues centre mainly on views from the rear of existing houses. While the Design and Access Statement Opportunities and Constraints analysis identifies sensitive boundaries to existing dwellings, the proposed space for and provision of 'buffer' perimeter planting on the Masterplan looks somewhat sparse.

The Masterplan indicates that it is proposed to retain some of the existing trees/hedges and provide new planting, including a fair amount of tree planting along some of the internal road network. Existing hedge lines in the South of the site would be incorporated within a green 'swathe'/corridor which would be a positive feature.

If the principle of development was accepted the following detail would need to be carefully addressed at Reserved Matters stage in finalising any layout - ensure realistic amenity standoff distances between retained and new trees and new houses; allow adequate space for perimeter planting to soften the development; provide an Arboricultural Method Statement and provide a detailed, high quality landscape scheme

- 7.4 Travelwise: In accordance with the SPD on Travel Plans the Travel Plan should be a required planning obligation along with the following:
- a) Leeds City Council Travel Plan Review fee of £2,650 b) provision of a Residential Travel Fund of £62,562.50 towards a residential travel plan fund for the provision of Travel Plan measures for the dwellings on the development and/or other sustainable travel measures to encourage the use of sustainable travel modes by the residents of the dwellings to accord with Core Strategy Policy T2 and c) conditions relating to the provision of cycle parking, electric charging points and the implementation of the Travel Plan.
- 7.5 Flood Risk Management: On the basis that the developer agree with a proposed rate of discharge set by the Council's FRM team, which the applicant has subsequently confirmed, FRM would do not have any objections to the proposed development.

- 7.6 Nature Conservation: The bat roost surveys are satisfactory and conclude that the building to be demolished does not have a bat roost at this time, and no trees on-site have significant bat roost potential. Bat commuting and foraging surveys have also taken place and are satisfactory to conclude that it is unlikely that the favourable conservation status of the species identified will be adversely impacted upon. It is noted that there are a number of locally valuable habitats that will be lost (semi-improved grassland and marshy grassland) and therefore conditions recommended to secure a Construction Environmental Management Plan and a Bio-diversity Enhancement Plan to help achieve no net loss of biodiversity as per the NPPF. A condition requiring a Method Statement to eradicate Japanese Knotweed is also proposed
- 7.7 Public Rights of Way (PROW): Public footpath No.147 Leeds runs along the eastern boundary of site. Although the footpath is to remain on its original line, it should be noted that the Rights of Way Review Committee Practice Guidance notes advise that if the way is to be enclosed by fencing, hedging or buildings then footpaths should be of a minimum width of 4 metres. Officers would like to see the footpath with a 2 metre wide surface within a 4 metre wide corridor. The surface should also be upgraded to a specification agreed with officers. Looking at the master plan, Officers note that the start of the footpath from Whitehall Road appears to run along the access road into the development. However, for safety reasons it would be advisable to divert the footpath onto a line through the public open space to the eastern side of the access road, which would be the safer option. They would also welcome the connection with the claimed footpath and the non-definitive footpath, which runs through the development site.
- 7.8 Children's Services: Cobden Primary School is the nearest school to this development. It is over-subscribed and the Council is currently in discussion with them to see if they can offer additional places on a temporary basis for this year and next to meet existing demand although a permanent expansion is unlikely due to Highways concerns. Although Children's Services would only expect the development to generate around 33 primary age pupils in total, or 5 per year group, this school would be unable to absorb any additional demand. There may be some available capacity to absorb this demand between Ryecroft Academy which is 1.7 miles walking distance from this site or at Gildersome Primary School which is also 1.7 miles away. Secondary place demand is increasing and discussions across the city are on-going to create extra capacity over the coming years.
- 7.9 West Yorkshire Archaeology: There are currently no apparent significant archaeological implications associated with the proposed development

8.0 PLANNING POLICIES

- 8.1 Section 38 (6) of the Planning and Compulsory Purchase Act 2004 requires that applications should be determined in accordance with the development plan unless material considerations indicate otherwise. The Development Plan for Leeds comprises the Adopted Core Strategy (2014), saved policies within the Leeds Unitary Development Plan (Review 2006) and the Natural Resources and Waste Development Plan Document (2013).
- 8.2 The site is identified on the LDF Policies Map as a Protected Area of Search.

Adopted Core Strategy

8.3 The following Core Strategy policies are considered most relevant:

Spatial Policy 1: Location of development
Spatial Policy 4: Regeneration Priority Programme Areas
Spatial Policy 6: Housing requirement and allocation of housing land
Spatial Policy 7: Distribution of housing land and allocations
Spatial Policy 11: Transport infrastructure investment priorities
Policy H1: Managed release of sites
Policy H3: Density of residential development
Policy H4: Housing mix
Policy H5: Affordable housing
Policy P10: Design
Policy P11: Conservation and Listed Buildings
Policy P12: Landscape
Policy T1: Transport Management
Policy T2: Accessibility requirements and new development
Policy G4: New Greenspace provision
Policy G8: Protection of species and habitats
Policy G9: Biodiversity improvements
Policy EN2: Sustainable design and construction
Policy EN5: Managing flood risk
Policy ID2: Planning obligations and developer contributions

Saved Policies - Leeds UDP (2006)

8.4 The following saved policies within the UDP are considered most relevant to the determination of this application:

GP5: Development Proposals should resolve detailed planning considerations.
N23/25: Landscape design and boundary treatment
N34: Protected Area of Search sites (PAS)
LD1: Detailed guidance on landscape schemes.

Submission Draft Site Allocations Plan (SAP) (February 2017)

8.5 Within the Submission Draft Site Allocations Plan, the application site (SAP reference HG3-17) is identified as a 5.7-hectare site with a capacity for 130 dwellings. It is identified as Safeguarded Land with Policy HG3 of the Submission Draft SAP stating that the SAP designates sites to be safeguarded from development for the Plan Period (to 2028) to provide a reserve of potential sites for longer-term development post 2028 and protect the Green Belt. The weight to be attached to the Submission Draft SAP (limited) is considered at Paragraph 10.12 below.

Relevant supplementary guidance:

8.6 Supplementary Planning Guidance provides a more detailed explanation of how strategic policies of the Unitary Development Plan can be practically implemented. The following SPGs are most relevant and have been included in the Local Development Scheme, with the intention to retain these documents as 'guidance' for local planning purposes:

New Farnley Village Design Statement (Adopted as an SPD in April 2013)
Street Design Guide SPD

National Planning Policy Framework (NPPF)

- 8.7 The National Planning Policy Framework (NPPF), published on 27th March 2012, and the National Planning Practice Guidance (NPPG), published March 2014 replaces previous Planning Policy Guidance/Statements in setting out the Government's planning policies for England and how these are expected to be applied. One of the key principles at the heart of the Framework is a presumption in favour of Sustainable Development.
- 8.8 The NPPF constitutes guidance for Local Planning Authorities and its introduction has not changed the legal requirement that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise.
- 8.9 The NPPF establishes at Paragraph 7 that there are three dimensions to sustainable development: economic, social and environmental of which the provision of a strong, vibrant and healthy community by providing the supply of housing required to meet the needs of present and future generations is identified as a key aspect of the social role. Within the economic role, it is also acknowledged that a strong and competitive economy can be achieved by ensuring that sufficient land of the right type is available in the right places and at the right time to support growth and innovation.
- 8.10 Paragraph 17 sets out twelve core planning principles, including to proactively drive and support sustainable economic development to deliver the homes, business and industrial units, infrastructure and thriving local places that the country needs, ensuring high quality design but also encouraging the effective use of land by reusing land that has been previously developed (brownfield land), provided that it is not of high environmental value.
- 8.11 With specific regard to housing applications, the NPPF states at paragraph 47 that to boost the supply of housing, local planning authorities must identify and update annually a supply of specific deliverable sites sufficient to provide five years' worth of housing against their housing requirements with an additional of 5% (moved forward from later in the plan period) to ensure choice and competition in the market of land. Deliverable sites should be available now, be in a suitable location and be achievable with a realistic prospect that housing will be delivered on the site within 5 years. It states that where there has been a record of persistent under delivery of housing, local planning authorities should increase the buffer to 20%.
- 8.12 Paragraph 49 of the National Planning Policy Framework states the following:

"Housing applications should be considered in the context of the presumption in favour of sustainable development. Relevant policies for the supply of housing should not be considered up-to-date if the local planning authority cannot demonstrate a five-year supply of deliverable housing sites."
- 8.13 In the appeal decision dated 8th June 2016 in relation to land at Grove Road, Boston Spa in accordance with APP/N4720/A/13/2208551, the Secretary of State took the

view that on the basis of the evidence available to him at that time, the Council was unable to demonstrate a deliverable 5-year supply of housing land. This conclusion has subsequently been reinforced by the Secretary of State decision(s) on the conjoined appeals at Breary Lane, Bramhope, Bradford Road, East Ardsley and Leeds Road, Collingham (the “Ken Barton Conjoined Appeals”), which were considered by Planning Inspector Ken Barton in Spring 2016. On 22nd December 2016, the Secretary of State issued his decision on these conjoined appeals and agreed with the Inspector’s conclusions that the appeals should be allowed. In reaching the decision on land at Bradford Road at East Ardsley (APP/N4720/W/15/3004034), which is representative of the other conjoined appeal decisions, the Secretary of State concluded the following (summarised):

Paragraph 11: The Secretary of State agrees with the Inspector that on past performance, the buffer must be 20% - so that the 5-year housing land supply requirement across the City would be 31,898 or 6379 units per annum.

Paragraph 12: The Secretary of State agrees with the Inspector’s conclusion that the failure [of Leeds City Council] to produce an Adopted SAP (Site Allocations Plan) until at least December 2017 means that there is no policy set out to show how delivery of any houses, never mind the magnitude required, will actually take place; that the safety margin of 2262 dwellings can soon be whittled away when realism is applied and that the Council has failed to demonstrate a robust 5 year housing land supply. The Secretary of State therefore agrees with the Inspector’s conclusion that the solution is to deliver housing now, including much needed affordable housing.

Paragraph 13: Having regard to the Development Plan position, the Secretary of State agrees with the Inspector that there is no 5-year housing land supply. Therefore, whilst he agrees with the Inspector that the UDPR policy N34, which designates sites as a Protected Area of Search (PAS) is a policy for the supply of housing, he also agrees with the Inspectors conclusion that policy N34 cannot be considered up-to-date. He further agrees with the Inspector that, rather than being a restrictive policy, the purpose of Policy N34 was to safeguard land to meet longer term development needs, so that, as it envisages development, the appropriate test to apply is whether any adverse impacts of granting permission would significantly and demonstrably outweigh the benefits when assessed against the policies in the Framework as a whole.

- 8.14 Accordingly, the Council is now in the position that it does not have a 5 year housing supply and the policies within the Unitary Development Plan and Core Strategy that are relevant to the supply of housing are considered to be out of date. In determining which policies are defined as ‘relevant policies for the supply of housing’, in terms of those policies that should be considered out-of date, case law has determined that Paragraph 49 should be interpreted widely and applies to all policies which are restrictive of where housing development can go. Paragraph 14 of the NPPF is, therefore, now particularly relevant, which states the following:

“At the heart of the National Planning Policy Framework is a presumption in favour of sustainable development, which should be seen as a golden thread running through both plan-making and decision-taking.

For decision-taking this means:

Approving development proposals that accord with the development plan without delay; and where the development plan is absent, silent or relevant policies are out-of-date, granting permission unless:

— any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole; or

— Specific policies in this Framework indicate development should be restricted.”

It is important to note that an ‘out of date’ policy does not become irrelevant and it is therefore the case that an assessment must be made in respect of the weight to be attached to such policies in the planning balance of decision making overall.

- 8.15 In relation to highway matters, Paragraph 32 of the NPPF states that all developments that generate significant amounts of movement should be supported by a Transport Statement or Transport Assessment. Plans and decisions should take account of whether: the opportunities for sustainable transport modes have been taken up depending on the nature and location of the site, to reduce the need for major transport infrastructure safe and suitable access to the site can be achieved for all people; and improvements can be undertaken within the transport network that cost effectively limit the significant impacts of the development. Development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe.
- 8.16 Finally, also of relevance to this application is guidance within the NPPF in relation to policy implementation and the status to be given to emerging plans. Paragraph 216 of the NPPF advises decision-takers may also give weight to relevant policies in emerging plans according to:
- (i) The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
 - (ii) The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
 - (iii) The degree of consistency of the relevant policies in the emerging plan to the policies in this Framework (the closer the policies in the emerging plan to the policies in the Framework, the greater the weight that may be given).

This is pertinent to the site allocation process in Leeds.

9.0 MAIN ISSUES

- 9.1 The main issues to consider in the determination of this application include the following:
- i. Principle of development – Policy and Land Use
 - ii. Housing Density and Housing Mix
 - iii. Affordable Housing
 - iv. Means of Access – Highways
 - v. Layout, Scale and Appearance (including Green Space)
 - vi. Landscaping

- vii. Residential Amenity
- viii. Ecology
- ix. Flood Risk
- x. Sustainability
- xi. Flood Risk
- xii. Demolition of the existing dwellings

9.2 The Council must also consider representations received as part of the public consultation exercise.

10.0 APPRAISAL

Principle of development

- 10.1 Within the January 2014 Policies Map, which comprises the Saved UDP Review 2006 policies and the Adopted Natural Resources and Waste Local Plan, the application site is identified as a Protected Area of Search for long-term development (PAS). On the Policies Map, the village settlement of New Farnley, including the application site, is shown as being surrounded by, but excluded from, the Green Belt. Members are also advised that a thorough review of all UDP PAS sites has been undertaken as part of the preparation of the Site Allocations Plan (SAP). As a result of a comprehensive comparative site assessment exercise, the Submissions Draft SAP does not propose that this land should be allocated for development, instead, proposing that it remains as safeguarded land. This is because, in terms of the site allocation process, other more sustainable and preferable sites are considered to be available to meet the needs over the plan period.
- 10.2 However, on the basis of the recent appeal decisions, Leeds City Council is unable to demonstrate a 5-year housing land supply and it is considered to be consistently under-delivering. The key assessment in determining this application is therefore the extent to which weight can be attached to the policies of the existing and emerging Local Plan in light of a shortfall in the 5-year housing land supply. Therefore, there needs to be a balancing exercise within the parameter that there is a presumption in favour of granting permission unless any adverse impacts would significantly and demonstrably outweigh the benefits, when assessed against the policies in the NPPF as a whole.
- 10.3 It is very clear from the Secretary of State's decision on the conjoined appeals noted above that UDPR Policy N34, which relates to Protected Area of Search sites, is a policy for the supply of housing and it cannot be considered up-to-date. The appeal decisions also confirm the Secretary of State's view that '*Policy N34 is now time expired and that its use to prevent development would be contrary to the terms of the Framework*'. Policy N34 must therefore attract little weight in the determination of this application.
- 10.4 Having regard to relevant policies within the Adopted Core Strategy, it is noted that the Core Strategy is up-to-date; it was published after the NPPF and was found to be sound. Accordingly, full weight can be attached to the distribution strategy for the appropriate location of development as set out in Core Strategy Spatial Policies SP1, SP6 and SP7.
- 10.5 Spatial Policy 1 of the Core Strategy relates to the location of development and confirms the overall objective is to concentrate the majority of new development

within and adjacent to urban areas, taking advantage of existing services, high levels of accessibility, priorities for urban regeneration and an appropriate balance between brownfield and greenfield land. It confirms that the largest amount of development will be located in the main urban area and major settlements with small settlements contributing to development needs subject to the settlement's size, function and sustainability. As a consequence, the priority for identifying land for development is (i) previously developed land within the Main Urban Area/relevant settlement, (ii) other suitable infill sites within the Main Urban Area/relevant settlement and (iii) key locations identified as sustainable extensions to the Main Urban Area/relevant settlement. New Farnley is considered to a village rather than a smaller settlement and falls within the definition of 'all other settlements'. Within the NPPF, the effective use of land by reusing brownfield land is encouraged but the development of Greenfield land is not precluded with the presumption in favour of sustainable development the primary determinant.

- 10.6 Clearly, the application site does not constitute previously developed land nor is it adjacent to the main urban area but it is considered that these facts, in themselves do not warrant a refusal as the determination of this application must be on the basis of a planning balance in the context of the shortfall in the 5 year housing supply. In this regard, it is also the case that the site is adjoined by existing housing on two sides to Whitehall Road/Walsh Lane and Castle Ings Gardens and almost a third of its boundary to Low Moor Side Lane is bounded by existing housing. Additionally, along with the existing dwellings, the site is excluded from the Green Belt, by which it is surrounded such that there is no potential to further extend the settlement of New Farnley beyond the boundaries of the existing housing and this site such that the site effectively forms an infill within the village settlement of New Farnley.
- 10.7 Spatial Policy 6 of the Core Strategy relates to the City's Housing Requirement and the allocation of housing land. It confirms that the provision of 70,000 (net) new dwellings will be accommodated between 2012 and 2028 with a target that at least 3,660 per year should be delivered from 2012/13 to the end of 2016/17. Guided by the Settlement Hierarchy, Spatial Policy 6 confirms that the Council will identify 66,000 dwellings (gross) (62,000 net) to achieve the distribution in tables H2 and H3 in Spatial Policy 7 (which identifies a need for 4700 new homes in the Outer West Housing Market Character Area within which the site is located, representing 7% of the City-wide distribution) using the following considerations:
- (i) Sustainable locations (which meet standards of public transport accessibility), supported by existing or access to new local facilities and services, (including Educational and Health Infrastructure);
 - (ii) Preference for brownfield and regeneration sites;
 - (iii) The least impact on Green Belt purposes;
 - (iv) Opportunities to reinforce or enhance the distinctiveness of existing neighbourhoods and quality of life of local communities through the design and standard of new homes;
 - (v) The need for realistic lead-in-times and build-out-rates for housing construction;
 - (vi) The least negative and most positive impacts on green infrastructure, green corridors, green space and nature conservation;
 - (vi) Generally avoiding or mitigating areas of flood risk.

In response to these considerations, the following is advised:

- 10.8 (i) In terms of a sustainable location, the accessibility of the scheme is considered fully in the Transport section below at Paragraph 10.23, which will acknowledge that the site does sufficiently meet the Accessibility Standards established at Table 2, Appendix 3 of the Core Strategy such that it is considered to be a sufficiently sustainable and accessible location with suitable access to local facilities and services. With regard to access to facilities and services, including education and health infrastructure, the matter of education is considered fully below at Section 11.0.
- 10.9 With regard to health infrastructure, the provision of health facilities falls within the remit of NHS England and at a local level, Leeds' three Clinical Commissioning Groups (CCGs). The amount of new housing identified for Leeds up to 2028 would equate to, on average, 5-6 new GPs a year across Leeds based on a full time GP with approximately 1800 patients. The Site Allocations Plan cannot allocate land specifically for health facilities because providers plan for their own operating needs and local demand. Existing practices determine for themselves (as independent businesses) whether to recruit additional clinicians in the event of their registered list growing. Practices can also consider other means to deal with increased patient numbers, including increasing surgery hours. It is acknowledged that the GP practice that previously existed within New Farnley (New Farnley Surgery on West End) is now closed and the next nearest practice is Wortley Beck Health Centre (circa 1 mile) and then Gildersome Health Centre (circa 1.3 miles), both of whom are currently accepting patients. The nearest dental practice accepting patients is at 268 Tong Road (Whingate Dental Care) at a distance of circa 1.7 miles such that there are health facilities available.
- 10.10 (ii) to (vi) Whilst it is a Greenfield rather than Brownfield site, neither Spatial Policy 6 nor the NPPF preclude the development of Greenfield sites. It is also clearly outside of the Green Belt and will therefore not impact upon it. The standards and design of the development, which will be determined at Reserved Matters stage, should offer the opportunity to enhance the distinctiveness of the locality and provide a high quality design standard for new homes having regard to the New Farnley Village Design Statement SPD. The impact with regard to nature conservation and flood risk have been fully considered and are addressed in the report below but none of these issues are considered to preclude development commencing in accordance with Spatial Policy 6.
- 10.11 With specific regard to the managed release of sites, Policy H1 of the Core Strategy confirms that the LDF Allocations Documents will phase the release of allocations according to the following five criteria to maintain a 5-year housing supply:
- i. Location in regeneration areas,
 - ii. Locations which have the best public transport accessibility,
 - iii. Locations with the best accessibility to local services,
 - iv. Locations with least impact on Green Belt objectives,
 - v. Sites with least negative and most positive impacts on existing and proposed green infrastructure, green corridors, green space and nature conservation.
- 10.12 As noted above, having regard to the Site Allocation Process it is acknowledged that within the Submission Draft SAP, the application site remains as safeguarded land. It is also acknowledged that the NPPF (paragraph 85) makes clear that safeguarded / PAS land is not allocated for development and that planning permission for its permanent development should only be granted following a Local Plan review which proposes the development. However, with reference to Paragraph 212 of the

NPPF, the Submission Draft SAP can, at this point in time, be afforded limited weight due to its stage of preparation. Accordingly, the determination of this application is a balancing exercise within the parameter that there is a presumption in favour of granting permission. As will be set out in the report below, the site is sufficiently accessible to local services, with accessibility to public transport and it will have very limited impact upon the Green Belt, being outside of the Green Belt. With regard to Policy H1 (v), it will also provide some improvements to publicly accessible green space by providing on-site public open space and ecological enhancements, also detailed in the report below.

Conclusion – principle of development

- 10.13 Policies SP1, SP6 and SP7 of the Core Strategy, which provide a framework for directing housing development to the most sustainable locations, are considered to be broadly consistent with the NPPF, and so the principle of the approach promoted by them may be given significant weight. However, the site-specific policies that affect this site, most notably policy N34 of the UDP and emerging policy HG3-17 of the SAP may only be attributed limited weight.
- 10.14 The presumption in favour of sustainable development means that planning permission must be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the policies in the NPPF taken as a whole. As will be demonstrated in the report below, the majority of the site is within suitable journey times (as established through Policy T2 of the Core Strategy) from a number of the key services and facilities. This, and the fact that the site is bounded on 3 sides by existing development and so its impact on the wider landscape is more limited and that there are no unresolved objections from other specialist consultees weighs in favour of the proposal.
- 10.15 Furthermore, whilst it is considered that it would be far more appropriate to the decision on whether development should take place on this site to be made through the SAP process, where it can be considered alongside all of the other sites that offer potential to help meet the need for new housing in the Outer West area over the plan period, when assessed against the PPG test for prematurity it is not considered that the application is premature. As a result of all of the above, it is determined that there are no grounds to refuse the application on the principle of development at the current point in time and a site specific analysis is required as set out in the report below. It must therefore be considered in the context of the presumption in favour of sustainable development and, in accordance with guidance within the NPPF, approved without delay unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the NPPF taken as a whole. This balancing exercise is considered at Paragraph 12 of this report following consideration of detailed matters set out below.

Housing Density and Housing Mix

- 10.16 Policy H3 of the Core Strategy relates to the appropriate density of development and advises that housing development in Leeds should meet or exceed the relevant net densities unless there are overriding reasons concerning townscape, character, design or highway capacity. In this case, as a 'smaller settlement' a minimum density of 30 dwellings per hectare would comply with Policy H3. The Design and Access Statement submitted to support this application envisages an average density of 25 dwellings per hectare based upon providing up to 130 dwellings, which is intended to have regard to character, design, highway capacity and the delivery of

on-site green space. It is therefore a matter that will be assessed fully at Reserved Matters stage.

- 10.17 Similarly, housing mix will also be assessed fully at Reserved Matters stage with the applicant needing to have regard to the preferred housing mix set out at Table H4 of the Adopted Core Strategy to comply with Core Strategy Policy H4. The applicant will be required to submit a Housing Needs Assessment at that time addressing all tenures so that the needs of the locality can be taken into account at the time of the development.

Affordable Housing

- 10.18 Policy H5 of the Core Strategy sets out the requirement for on-site affordable housing, which is expected to comprise 15% of the development in this part of the City to be secured by means of a planning obligation via a Section 106 Legal Agreement. On a development of 130 houses, the Council would expect 20 of those to be identified for affordable housing, 40% of which should be disposed of to households on lower quartile earnings and 60% to households on lower decile earnings. The proposed development is therefore in accordance with Policy H5.

Housing for Independent Living

- 10.19 Policy H8 of the Core Strategy advises that developments of 50 or more dwellings are expected to make a contribution to supporting needs for independent living such as including the provision of bungalows or level access flats. The applicant is aware of the requirement and this will be assessed as part of the Reserved Matters submission.

Means of Access – Highways

- 10.20 Notwithstanding the principle of development, the means of access into the site is the sole matter for determination as part of this application. With reference to the Development Plan, Policy T2 of the Core Strategy advises that new development should be located in accessible locations and with safe and secure access for pedestrians, cyclists and people with impaired mobility with appropriate parking provision. Appendix 3 of the Core Strategy also sets out accessibility standards for development. The NPPF seeks to support sustainable transport solutions but it advises at Paragraph 32 that development generating significant movements should be supported by a Transport Assessment and that development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe.

Means of Access

- 10.21 The application proposes a single vehicular access from Whitehall Road. The access requires the demolition of 634 Whitehall Road and 632 Whitehall Road to enable a new access road of an appropriate 5.5-metre width with 2 metre footways on each side. The access is designed with a right turn lane from Whitehall Road. Following discussions with the Council's Traffic, Road Safety, Cycle Team and the Abnormal Loads Officer, it is concluded that the proposed access arrangements are acceptable and they offer a safe and efficient means of accessing the site in accordance with Core Strategy Policy T2.

- 10.22 The detailed access provision within the site will be determined at Reserved Matters stage albeit that the submitted Illustrative Masterplan indicates an internal access road served from the proposed access onto Whitehall Road, which runs between 630 and 636 Whitehall Road and then to the rear of the gardens of 630 Whitehall Road and 72 and 74 Castle Ings Gardens. To protect the residential amenity of these adjoining occupiers the access road has a landscaped buffer of between 6 and 13 metres between the back edge of the pavement and the side boundaries of 630 and 636 Whitehall Road and with the exception of a small 'pinch point' in relation to an additional piece of garden space to the rear of 636 Whitehall Road that projects into the site, a buffer of between 3 metres and 15 metres to the rear gardens of 72 and 74 Castle Ings Gardens. The internal access road is currently designed as a series of 5 cul-de-sacs
- 10.23 With regard to accessibility, Core Strategy Policy T2 refers to Accessibility Standards, which are set out at Appendix 3 of the Core Strategy. They are based upon an average walking speed of 3 miles per hour. In relation to this site, the Accessibility Standards comprise the following:

Destination	Standard	Compliance of this site
To Local Services	Within a 15-minute walk.	The Co-Operative food store and Post Office on Low Moor Side Lane are within a 15-minute walk such that local services are accessible. The nearest Doctors Surgery is circa 1 mile from the site; it is accessed via three bus routes along Whitehall Road followed by a 0.4 mile walk such that it does not directly meet the standard of having a GP within a 20 minute walk or a 5 minute walk to a bus stop with a direct service at a 15 minute frequency albeit that the bus frequency is much greater but not direct.
To Employment	Within a 5 minute walk to a bus stop offering a 15 minute frequency to a major public transport interchange Or, within a 40 minute journey time	A small proportion of the site in the north-west corner is just beyond 400 metres/5 minutes walk of a bus stop but at least 75% of the site (at more reasonably, circa 80% of the proposed dwellings) are within 400-metres/5 minute walk of bus stops on A58 Whitehall Road. During the weekday and Saturday, the services (Nos.209, 252, 254, 255 and 225 (westbound only) combine to provide an overall hourly two-way frequency of 12 buses per hour with 6

		buses and hour on a Sunday, equating to one bus every 5 minutes Monday to Saturday and one bus every 10 minutes on Sundays with connections to Leeds, Halifax, Wakefield, Cleckheaton, Heckmondwike and Brighouse.
To Primary Education and Health	Within a 20 minute walk or a bus stop offering a direct service at a 15 minute frequency	Lawns Park Primary School is identified to be within a 25 minute walk of the entire site. Lower Wortley and Cobden Primary Schools are within a 20-minute journey via the bus stops on Whitehall Road.
To Secondary Education	Within a 30 min direct walk or 5 min walk to a bus stop offering a 15 min frequency to a major public transport interchange	Farnley Academy is within a 20-minute walk from the site.
To Town Centres/City Centre	Within 5 min walk to a bus stop offering a 15 min frequency service	Circa 75% -80% of the development is within 5 minutes of a bus stop with a 5-minute frequency service to the town centres above and Leeds City Centre.

- 10.24 In terms of compliance with the Council's Accessibility Standards, it is recognised that a small proportion of the development is more than 5 minutes walk from a bus stop with a 15 minute frequency, thus affecting accessibility principally to employment and town/city centres. The applicant notes that the bus stops on Whitehall Road receive a service frequency that far exceeds the minimum provision of a 15-minute frequency service and in fact, there are 12 buses per hour during weekdays and on Saturdays and 6 on Sunday.
- 10.25 However, it is considered that the site's accessibility to goods and services forms part of the overall balancing exercise, which is considered at Paragraph 12 of this report. Moreover, Policy T2 does not state that compliance with Appendix 3 is a requirement of meeting the policy but rather that new development should be located in accessible locations that are *adequately* served by existing or programmed highways, by public transport and with safe and secure access for pedestrians, cyclists and people with impaired mobility. Additionally, the test established in the NPPF with regard to highway matters is that development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe. On balance, it is therefore considered that failure of part of the site to comply fully with the accessibility standards, particularly taking into account bus service frequency generally, would not itself warrant a recommendation of refusal. A failure to entirely meet the Accessibility Standards has recently been tested at the PAS Public Inquiries for Brearly Lane, Bramhope,

Bradford Road, East Ardsley and Leeds Road, Collingham. The Inspector and Secretary of State dismissed the Council's accessibility concerns at these sites.

Transport Assessment and Mitigation

- 10.26 The application includes the submission of a Transport Assessment to consider the highway impact of the proposed development on the basis of up to 130 dwellings.
- 10.27 As part of their original submission, the applicant assessed the impact of the development by producing a bespoke vehicle trip rate based on surveys of a nearby residential estate (Beechfield). This was achieved using actual traffic surveys and an overall average of trips recorded on Tuesday, Wednesday and Thursday of the survey period. Whilst there were no objections in principle to this methodology, it was considered that the precise demographics and travel characteristics of the two sites may be different. There is no indication of the size of houses to be built whilst the surveyed housing estate is mainly 3 bed detached and semi-detached, and therefore, it was considered that this may not be comparable if large 4 – 5 bedroom dwellings were proposed. Accordingly, the applicant was asked to validate the proposed trip rate against TRICS data for similar sites to ensure that a suitable trip rate is being used to predict the impact of the development on the highway network. This showed that the surveyed trip rates were higher than both the average trip rates calculated from the TRICS database and Officers are therefore satisfied that the surveyed trip rates from the Beechfield estate used in the TA are robust and that the traffic generation predicted is accurate and representative. This equates to a total of 13 arrivals and 53 departures in the AM Peak (0800 to 0900) and 50 arrivals and 26 departures in the PM peak (1700 to 1800). The modeling submitted within the Transport Assessment with regard to highway impact has been fully assessed by the Council's Urban Traffic Control team and they are content that the proposals would not be detrimental to the safe operation of the highway such that there is sufficient capacity on the highway network to accommodate this development in accordance with Core Strategy Policy T2.
- 10.28 Overall, it is therefore concluded that the proposal is acceptable in highway terms. Subject to relevant conditions and the requirements of the Section 106 Legal Agreement, it is concluded that the proposed development is located in a sufficiently accessible location and it will provide a safe and secure access for pedestrians, cyclists and people with impaired mobility with appropriate parking provision such that the means of access is acceptable. On this basis, the development is also not considered to result in a severe residual cumulative highway impact to warrant a refusal such that it must be concluded that the proposed means of access is acceptable and the development is in accordance with Policy T2 of the Core Strategy and guidance within the NPPF

Layout, Scale and Appearance (including Green Space)

- 10.29 Core Strategy Policy P10 reinforces the requirement for new development that is based on a thorough contextual analysis to provide good design that is appropriate to its scale and function; that respects the scale and quality of the external spaces and wider locality and protects the visual, residential and general amenity of the area. Within the UDP, Saved Policy BD5 advises that new buildings should be designed with consideration of their own amenity. These policies reflect guidance within the NPPF. In this case, matters of layout, scale and appearance are reserved for future consideration at the Reserved Matters stage and are not part of the assessment of this outline application. However, this application submission

includes an Illustrative Plan and Design and Access Statement, which provides an indication of the form of future landscaping and development.

Layout

- 10.30 The detailed layout and the relationship between existing housing and the proposed new housing will be fully considered at Reserved Matters stage. It is noted that the applicant has submitted an illustrative Masterplan as part of this outline application, as well as illustrative site sections, which indicate the relationship to the existing houses around the site. The site sections take into account the topography of the site. However, it is advised that neither the illustrative Masterplan nor the site sections would form part of the approval of this application and the layout will necessarily be fully assessed against the amenity and privacy standards established within the Council's Neighbourhoods for Living SPG at Reserved Matters stage.
- 10.31 With regard to the provision of green space within the site, which will also influence the layout, Policy G4 of the Core Strategy requires the provision of 80 square metres of green space per dwelling where they are in excess of 720 metres from a community park and for which are located in areas deficient of open space, which is in effect, the entire City. This is a requirement secured by a planning obligation via the Section 106 agreement. Should the site be developed for 130 houses, this would equate to a greenspace requirement of 1.04 hectares. The Design and Access Statement and illustrative Masterplan currently identify only 0.57 hectares of open space, which falls below the requirements of Core Strategy Policy G4. However, the exact provision of open space in accordance with Core Strategy G4 will be determined at Reserved Matters stage.

Scale

- 10.32 The submitted Design and Access Statement advises that in determining the appropriate scale of development, consideration will be given to the character of the surrounding area albeit that the illustrative drawings indicate 2-storey dwellings, which is certainly the predominant character on Whitehall Road and Walsh Lane with Castle Ings Gardens being predominantly bungalows/dormer bungalows. Nevertheless, the detailed scale of the dwellings will be fully considered at Reserved Matters stage to take account of topography, residential amenity and design.

Appearance

- 10.33 The appearance of the dwellings will also be determined at the Reserved Matters stage to ensure that it is a development that is based on a thorough contextual analysis to provide good design that is appropriate to its scale and function in accordance with Policy P10 and guidance within the NPPF.
- 10.34 Overall, it is concluded that matters of layout, scale and appearance will be considered at the Reserved Matters stage but there is sufficient scope within the site and sufficient detail within the Design and Access Statement to ensure that a scheme can be delivered to meet the Council's design aspirations established within Core Strategy Policy P10, guidance within the NPPF and guidance within the Council's Neighbourhoods for Living SPG.

Landscaping

- 10.35 Policy P12 of the Core Strategy advises that the character, quality and bio-diversity of Leeds' townscapes and landscapes will be conserved and enhanced. Within the UDP, Policy LD1 provides advice on the content of landscape schemes, including the protection of existing vegetation and a landscape scheme that provides visual interest at street level.
- 10.36 In this case, landscaping is reserved for future consideration as part of a Reserved Matters submission. However, the submitted Design and Access Statement does establish a landscape strategy, which includes the intention to make the most of the existing landscape, vegetation, habitat and topography and to integrate the development within its surroundings as well as to promote bio-diversity. In terms of retaining the existing landscape features, bearing in mind the TPO across the site, the landscape strategy notes that the main group of hedgerows within the site will be retained and brought into positive management. In addition, the trees along the western site boundary to Low Moor Side Lane and the oak and birch trees adjacent to 630 Whitehall Road will also be protected. The parameters plan submitted with the application does indicate the removal of some trees to create the means of access into the site but this is mainly self-seeded scrub (Goat Willow/Pivot/Hawthorn) from within the site, which is determined to be of low quality. The landscape strategy does note that it is proposed to remove remnants of an existing hedgerow in the northern part of the site, which is identified as fragmented, to be replaced with hedge planting of an appropriate native species within the area of open space.
- 10.37 The Council's Landscape Officer has advised that landscape details will need to be carefully addressed at Reserved Matters stage in finalising any layout to ensure realistic amenity standoff distances between retained and new trees and new houses; allow adequate space for perimeter planting to soften the development and to secure a detailed, high quality landscape scheme as an Arboricultural Method Statement. It is proposed that these details be secured by conditions.
- 10.38 Subject to the above, it is considered that a successful landscape scheme can be established in accordance with the objectives of Core Strategy Policy P12 and UDP Policy LD1 subject to the above conditions and the details to be submitted as part of a Reserved Matters Landscape submission.

Residential Amenity

- 10.39 Policy GP5 of the UDP advises that development proposals should resolve detailed planning considerations including seeking to avoid problems of loss of amenity. The application site does adjoin existing residential development to the Accordingly, a detailed assessment of garden lengths and window to window distances will be undertaken at Reserved Matters stage, as well the imposition of conditions to ensure that means of enclosure, existing and proposed level changes within the site and any additional planting are also appropriate and adequate between existing and proposed properties. In view of the above, it is considered that a scheme can be developed at Reserved Matters stage that will comply with the requirements of Saved UDP Policy GP5 in terms of impacts on residential amenity.

Ecology

- 10.40 Policy G8 of the Core Strategy advises that enhancements and improvements to bio-diversity will be sought as part of new developments. These policies reflect advice within the NPPF to contribute to and enhance the natural and local

environment. Paragraph 118 of the NPPF advises that when determining planning applications, local planning authorities should aim to conserve and enhance biodiversity.

- 10.41 The application includes the submission of a Preliminary Ecological Appraisal, which includes bat surveys. The appraisal notes that the site comprises a complex of fields, predominantly species-poor semi-improved grassland which has been disturbed through furrowing. Small patches of neutral semi-improved grassland are also present. The habitats within the site are determined to be largely of low conservation value, comprising species-poor grassland habitats and areas dominated by vegetation and bracken that consist of common species, widely replicated within the surrounding area. It is noted that the scrub, tall vegetation and trees/shrubs to the north-western boundary and the native hedgerows, which partially dissect the site, all offer nesting opportunities for a number of bird species as well as providing potential invertebrate habitat.
- 10.42 Recommendations within the Ecological Appraisal include the protection of existing site features and mitigation for the loss of any on-site habitat and to try and enhance site biodiversity include the retention of hedgerows and trees wherever possible (which is largely proposed), planting of native species to the boundaries of the site where these are absent. Wildflower seeding in association with existing and new hedgerows using a mix suitable for woodland edges and semi-shaded spots is also recommended.
- 10.43 In terms of bats, the property at 632 Whitehall Road, which will be entirely demolished, has been the subject of a bat survey and no signs of bats were noted. It has also been determined that there are no mature trees within the site and therefore no adverse impact upon roosting bats within trees is anticipated as a result of the development.
- 10.44 It is considered likely that a large variety of bird species would utilise habitats within the site to nest including hedgerows, areas of dense scrub and trees and hedgerows, which will largely be retained within the proposed development. It is also recommended that new areas of native tree and shrub planting are introduced to maintain suitable breeding habitat for bird species currently using the site.
- 10.45 To address the recommendations of the Ecological Appraisal a condition to protect existing trees and hedging (with the exception of the fragmented hedging indicated for removal in the north of the site) is proposed as well as a condition seeking details of lighting and to avoid illuminating the site boundaries and any new areas of planting. A further condition to seek details of measures to enhance bio-diversity within the site is proposed as well as a method statement for the eradication of Japanese knotweed. Subject to these conditions, it is concluded that the proposed development has the potential to provide the opportunity to conserve and enhance bio-diversity in accordance with Policy G8 and guidance within the NPPF.

Flood Risk

- 10.46 Policy ENV5 of the Leeds Core Strategy advises that the Council will seek to mitigate and manage flood risk by (as relevant in this case), reducing the speed and volume of surface water run-off as part of new-build developments.
- 10.47 The site is located within Flood Zone 1 of the Environment Agency's indicative flood map and as such, it is considered to be at a low risk of flooding. However, due to

the size of the site in excess of 1ha, the application includes the submission of a Flood Risk Assessment. The submitted FRA confirms that the site is in Flood Zone 1 and also that the Environment Agency surface water flood risk mapping shows that there are localised areas of low surface water flood risk in the centre of the site and on the eastern boundary. The development will result in a positive drainage scheme to manage drainage across the site to include some on site surface water storage to the existing combined sewer in Whitehall Road as well as some attenuation storage to take account of climate change. Floor levels will also be set typically 150mm above immediate surrounding ground levels which will provide mitigation against any overland surface water flooding from extreme events. It is proposed that foul flows connect to the existing public combined sewers in the vicinity of the site.

- 10.48 The Council's Flood Risk Management Team have advised that the FRA and Drainage Strategy is generally acceptable subject to the developer accepting a specific rate of discharge to ensure that there is no increase in the volume of run-off from development sites for a range of storm durations over a given period. Flood Risk Management does not therefore have any objections to the proposed development subject to the imposition of a specific condition detailing the surface water drainage works. On this basis, it is concluded that the scheme will manage and mitigate flood risk in accordance with Policy ENV5 and guidance within the NPPF.

Sustainability

- 10.49 Core Strategy Policy EN1 requires that all developments of 10 dwellings or more will be required to reduce total predicted carbon dioxide emissions to achieve 20% less than the Building Regulations and provide a minimum of 10% of total energy needs from local carbon energy. Policy EN2 then requires all developments of 10 or more dwellings to achieve Code Level 4 from 2013 and Code Level 6 from 2016. Following a fundamental review of technical housing standards the Government has withdrawn the Code for Sustainable Homes with effect from 27th March 2015 such that the objectives of Policy EN2 will not be sought. The applicant has advised within the Design and Access Statement that a key sustainable principle in the delivery of sustainable housing is the usage of the 'fabric first' approach as these thermal performance considerations will affect the building throughout its lifetime. However, a condition requiring the applicant to provide a minimum of 10% of total energy needs from local carbon energy to comply with Policy EN2 will be sought as a condition of this recommendation.

Demolition of 632 and 634 Whitehall Road

- 10.50 The creation of the new vehicular access into the application site will certainly require the demolition of 632 Whitehall Road and also the demolition of 634 Whitehall Road. Since April 2011, the demolition of a building such as these dwellings constitutes development such that it forms part of the consideration of this application. The dwellings comprise a pair of detached post-War bungalows constructed in red brick and white render with a hipped roof that form part of the ribbon development. Whilst they are both attractive properties in good condition, they are not considered to be of any particular architectural merit; they are neither Listed nor within a Conservation Area to warrant consideration as an undesignated heritage asset. They do, however, constitute family housing, which will be lost as a result.

- 10.51 It is acknowledged that the buildings are in close proximity to existing residential properties such that their demolition will have to be carefully managed to protect the amenity of adjoining residents, with particular regard to noise and dust. However, in this regard, it is noted that demolition also requires compliance with the Building Act 1984 and in issuing a Demolition Notice, it is the case that a number of conditions normally have to be complied with during the demolition works necessary to maintain public safety and public amenity such that this issue of amenity in relation to demolition is a matter dealt with under other legislation. In addition, a Construction Management Plan is also proposed as part of this application to protect the amenity of adjoining residents during the construction period. It is therefore considered that no objection to their demolition can be sustained in this instance.

11.0 PLANNING BALANCE

- 11.1 In this case, although the application site is a Protected Area of Search (PAS), it is part of a pool of land, which was considered to offer the potential to meet longer-term development needs. UDP Policy N34 is a policy for the supply of housing, as has been found in the recent appeal decisions mentioned above. Furthermore, as there is no 5 Year Housing Land Supply in Leeds, the policy cannot be considered up to date and Paragraph 14 of the NPPF must be considered. The Inspector has found that rather than being a restrictive policy, at paragraph 85 of the NPPF, bullet points 3 and 4 specifically relate to safeguarded land, which, whilst not allocated at the present time, meets longer term development needs. The test that then applies is whether any adverse impacts of granting permission significantly and demonstrably outweigh the benefits, when assessed against the policies in the framework as a whole. The conclusion of this test will be a material consideration to be weighed in the balance when considering whether material considerations exist to outweigh the presumption in favour of the development plan in accordance with Section 38(6).
- 11.2 Considerable weight must be attached to the fact that this application will make a reasonable contribution to housing supply within the City providing up to 130 units at a time when the Secretary of State has determined in the recent appeal decisions that the 5-year housing land supply requirement across the City is 6379 units per annum. It will therefore contribute to providing a supply of housing required to meet the needs of future generations and importantly, include a supply of affordable housing, which at 15% provision of the maximum of 130 homes would equate broadly to 20 units.
- 11.3 In terms of location of the development, whilst this is a Greenfield site within a village settlement outside of the main urban area, it must be acknowledged that the development of greenfield sites is not precluded by either the Core Strategy or the NPPF. Importantly, this site is also distinguished by the fact that it represents an infill development within the settlement of New Farnley being adjoined by existing housing on two sides to Whitehall Road/Walsh Lane and Castle Ings Gardens and almost a third of its boundary to Low Moor Side Lane. Along the remainder of Low Moor Side Lane the site is set back behind a substantial grass verge with planting along the boundary such that the rural character of the Lane is largely retained. Additionally, the site is excluded from the Green Belt by which it is surrounded such that there is no potential to further extend either the settlement of New Farnley beyond the boundaries of the existing housing and this site, which provides a natural limit to the settlement.

- 11.4 In terms of social and environmental factors, it is noted that this proposal will result in the payment of the Community Infrastructure Levy, which although not a material consideration, could be utilised for a range of benefits including contributing towards secondary education provision, green infrastructure or public realm improvements. It will also result in the creation of a new area of publically accessible green space within the site. Subject to the imposition of appropriate planning conditions, it is determined that the proposal has the capacity to sufficiently protect and enhance the bio-diversity on site, as set out in the report above, introduce positive drainage onto the site to ensure that there is no flood risk and require that the houses are adapted to climate change through Building Regulations (fabric first) and the provision of 10% of energy needs from low carbon energy
- 11.5 In terms of potential adverse impacts, it is acknowledged that for local residents that adjoin the site, the development will result in a visual change to the landscape from the existing open fields and their existing views. However, the right to a view is not a material planning consideration and with regard to their residential amenity, to include matters such as privacy and outlook, the application will be fully assessed at Reserved Matters stage to ensure that privacy and amenity distances between existing and proposed dwellings are sufficient and have due regard to the existing character but there is sufficient site capacity to ensure that such matters can be appropriately addressed.
- 11.6 It is also acknowledged that the proposed development will result in some increase to traffic movements within the locality but it is not to the extent to constitute a severe cumulative impact. However, it will also bring about infrastructure improvements in terms of £30,000 to introduce a 20mph zone on neighbouring roads. Overall, it is concluded that on balance, these adverse impacts do not significantly and demonstrably outweigh the benefits of bringing the site forward to deliver housing and on that basis, the site is considered sustainable and in accordance with the NPPF. Thus, the presumption should be to approve without delay.

12.0 RESPONSE TO REPRESENTATIONS

- 12.1 The objections from local residents raise five key objections, which are largely addressed within the report above but the following key points are noted:
- a. In response to residents' concerns that the field was supposed to be protected until 2028 as a PAS site – whilst it is safeguarded within the Submission Draft Site Allocations Plan, this can only be afforded limited weight at this time such that this application currently has to be assessed on a site-specific basis having regard to the presumption in favour of sustainable development.
 - b. With reference to the New Farnley Village Design Statement, which has been adopted as an SPD by Leeds City Council, the intention of the Design Statement is to 'identify the local distinctiveness of New Farnley, encourage improvement where it is needed and aims to protect the best of what is there now'. It is a Design Statement rather than a Neighbourhood Plan such that it advises developers that it should be used to find out what is important in the area they are working in as new development will have an impact on the appearance of the area. Accordingly, the Design Statement cannot be used as a reason to refuse this application in principle but rather, it will be important at the Reserved Matters stage to guide the layout, scale and appearance of the

development to ensure that the development will enhance the character of the area.

- c. In terms of the view of residents that there are more suitable plots of brownfield land to build upon and greenfield sites should be preserved; there is no presumption in favour of developing brownfield land over greenfield; the NPPF encourages the re-use of brownfield land but does seek to exclude greenfield development.
- d. The assertion of residents that the service road fronting properties 590-658 Whitehall Road is a private road, owned and maintained (through insurances) by the residents and the developer does not have any rights over this road is a civil matter between the applicant and the residents; the matter does not preclude the determination of this application.
- e. In response to the concern from Councillor Blackburn that she is against any pedestrian links into the site as proposed on Low Moor Side Lane, it is the view of Officers that introducing pedestrian and cycle links across the site is a positive attribute of the development to improve local footpath connections and to enhance pedestrian accessibility throughout the area. The footpath would adjoin Low Moor Side Lane at a point where there is a footway such that it is considered to deliver an appropriate connection.

13.0 PLANNING OBLIGATIONS AND COMMUNITY INFRASTRUCTURE LEVY

- 13.1 The Community Infrastructure Levy (CIL) Charging Schedule was adopted on 12th November 2014 with the charges implemented from 6th April 2015 such that this application is CIL liable on commencement of development at a rate of £45 per square metre of chargeable floorspace. Due to the outline nature of this application, the floorspace is unknown at this stage. In any event, consideration of where any Strategic Fund CIL money is spent rests with Executive Board and will be decided with reference to the Regulation 123 list.
- 13.2 There is also a requirement for site-specific requirements to be secured via a Section 106 agreement as detailed below and the various obligations will become operational if a subsequent reserved matters application is approved and implemented:
 - i. Affordable Housing – 15% (with a 60% social rent and 40% submarket split);
 - ii. A contribution of £30,000 towards the creation of a 20mph speed limit on the neighbouring highways;
 - iii. Public open space on site of the size to comply with Core Strategy Policy G4;
 - iv. Provision of a Sustainable Travel Fund of £62,562.50;
 - v. Travel Plan Review fee of £2,650.
- 13.3 From 6th April 2010 guidance was issued stating that a planning obligation may only constitute a reason for granting planning permission for development if the obligation is:
 - (i) Necessary to make the development acceptable in planning terms – Planning obligations should be used to make acceptable, development which otherwise would be unacceptable in planning terms.

(ii) Directly related to the development - Planning obligations should be so directly related to proposed developments that the development ought not to be permitted without them. There should be a functional or geographical link between the development and the item being provided as part of the agreement.

(iii) Fairly and reasonably related in scale and kind to the development – Planning obligations should be fairly and reasonably related in scale and kind to the proposed development.

All contributions have been calculated in accordance with relevant guidance, or are otherwise considered to be reasonably related to the scale and type of development being proposed.

14.0 CONCLUSION

- 14.1 This application seeks outline planning permission for up to 130 dwellings to consider the principle of the development and means of access into the site only. Matters of appearance, landscaping, layout and scale are reserved for future consideration.
- 14.2 The application site is identified as a Protected Area of Search (PAS) on the UDP Policies Map and it also remains as safeguarded land within the Submissions Draft SAP as other more sustainable and preferable sites are considered to be available to meet the needs over the plan period. However, at this point in time, Policy N34, is time expired, conflicts with the objectives of the NPPF and can be afforded little weight. Similarly, due to its stage of preparation, the Submission Draft of the Site Allocations Plan (SAP) can also be afforded little weight. On this basis, whilst it would be preferable to determine whether development should take place on this site through the SAP process, it is considered there are insufficient grounds to refuse the application in principle at the current point in time and the assessment should be site-specific in the context of the presumption in favour of sustainable development.
- 14.3 It is accepted that the application does not fully meet the aspirations of Core Strategy Policy H1 or SP6 in terms of directing the majority of new development within and adjacent to the main urban area and seeking to encourage the re-use of brownfield land. But neither the Core Strategy nor the NPPF specifically exclude development on greenfield land outside of the main urban area and in this case, the site is distinguished by the fact that it represents an infill development within the settlement of New Farnley as set out in the report above. Of significant weight, however, is the fact that the scheme will bring forward up to 130 new dwellings to include 15% affordable housing and the fact that the means of access is considered to be safe and without any significant detriment to the adjoining highway. It is also considered to be sufficiently accessible to local services and facilities in accordance with the Council's Accessibility Standards such that it is on balance, considered to represent a sustainable development with a presumption in favour of such development clearly expressed within the NPPF.
- 14.4 The planning balance exercise is set out at Section 12 of this report where it concludes that any adverse impacts arising from this proposal are not considered to significantly and demonstrably outweigh the benefits of bringing the site forward to deliver housing and it is considered to represent a sustainable development. Therefore, having taken all representations received into account and given the compliance of this application relevant Policies within the Core Strategy, including

Spatial Policy 6 and 7, Policy H2, H3, T2, EN2, G8, P10 and ENV5 of the Core Strategy as well as Saved Policy GP5 of the UDP, it is on this basis, subject to conditions and a Section 106 Legal Agreement, that the application is recommended for approval.

Background Papers:

Application and history files.
Certificate C signed by the agent.



P14 4907 SK21 / ILLUSTRATIVE MASTERPLAN - NEW FARNLEY



ARCHITECTURE | PLANNING | LANDSCAPE

CLIENT:	REDROW HOMES & PARK LANE HOMES	DRAWING NUMBER:	P14 4907 SK21
PROJECT:	WHITEHALL ROAD, NEW FARNLEY	SCALE @ A1:	1:1000
DRAWING:	ILLUSTRATIVE MASTERPLAN	DRAWN:	DNH
		CHECKED:	LM
		DATE:	FEB 17

14 MARINER COURT / CALDER PARK / WAKEFIELD / WF4 3PL
01924 383222 / www.jpssoc.co.uk / info@jpssoc.co.uk

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Originator: Carol
Cunningham
Tel: 0113 378 7964

Report of the Chief Planning Officer -

SOUTH AND WEST PLANS PANEL

Date: 23rd March 2017

Subject: Application number 16/06222/OT – Outline application for residential development (Use Class C3) for up to 100 dwellings and land reserved for primary school with construction of vehicular access from Otley Road, to the north west and Ash Road to the south, areas of open space, landscaping, ecology treatments and associated works at Land to the East of Otley Road, Adel

APPLICANT

Hallam Land Management
Ltd and Barrett David Wilson
Homes

DATE VALID

5th October 2016

TARGET DATE

25th March 2017

Electoral Wards Affected:
Adel and Wharfedale

☐ Yes

Ward Members consulted
(referred to in report)

Specific Implications For:

Equality and Diversity ☐

Community Cohesion ☐

Narrowing the Gap ☐

RECOMMENDATION: DEFER and DELEGATE to the Chief Planning Officer for approval subject to the specified conditions set out below and also the completion of a Section 106 agreement to include the following obligations:

Financial contribution towards improvement works at Church Lane/Farrar Lane/Otley Road junction of £100,000 prior to the occupation of the 50th dwelling
Sustainable Travel Fund of £481.25 per unit
Travel plan Monitoring Fund
Bus Stop Contribution (£20,000)
On site provision of greenspace and maintenance
Affordable housing at 35%
Land to be reserved for school

In the circumstances where the Section 106 has not been completed within 3 months of the Panel resolution to grant planning permission, the final determination of the application shall be delegated to the Chief Planning Officer

1. Time limit on outline permission
2. Development in line with approved plans
3. Samples of walling and roofing materials to be submitted
4. Samples of surfacing materials to be submitted
5. Details of door and window frames to be submitted
6. Feasibility study into use of infiltration drainage to be submitted
7. Details of surface water drainage to be submitted
8. Development shall not commence until flood mitigation measures have been agreed which should be generally in accordance with the Flood Risk Assessment submitted
9. Details of site access opposite Kingsley Drive as a priority cross roads junction along with bus stop relocation and moving of speed limit to be submitted and implemented prior to first occupation
10. Details of extension of eastern footway on Otley Road from site access to existing footway on Holt Avenue to be submitted and implemented prior to first occupation
11. Details of provision of formal crossing on Otley Road in the vicinity of footpath 17 to be submitted and implemented prior to first occupation
12. Details of upgrade of public right of way 17 to be submitted and implemented in line with agreed timescale
13. Details of traffic calming on Church Lane shall be submitted and implemented before first occupation
14. There shall be no construction traffic from Ash Road
15. Details of provision of contractors during construction shall be submitted
16. No more than 36 residential units shall be accessed by vehicles from Ash Road and upon completion of the whole development this access shall be closed for vehicle use.
17. Provision for contractor during construction
18. Vehicles spaces to be laid out
19. Details of cycle/motorcycle parking
20. Details of external storage to be submitted
21. There shall be no built development on the eastern side of the Beck except for attenuation pools and the school playing pitch
22. Phase 2 site investigation to be submitted
23. Amendment of remediation statement if required
24. Submission of verification reports
25. Details of importing soils to be submitted
26. Details of existing and proposed ground levels to be submitted
27. Submission and implementation of landscaping
28. Arboricultural method statement
29. Landscape management plan
30. Protection of existing trees/hedges/bushes during construction
31. Preservation of retained trees/hedges/bushes
32. Provision for replacement trees/hedges/bushes
33. Details of fencing and walling to be submitted
34. Details of proposed construction hours to be submitted
35. Details to prevent noise, dust and odour to be submitted
36. No site clearance of vegetation during bird nesting season of March to September without bird nesting survey
37. Submission and implementation of a programme of archaeological recording
38. Details of noise protection from A660 shall be submitted
39. Details of bat protection and mitigation to be submitted
40. Details of provision of bat and bird boxes
41. Biodiversity Enhancement and Management Plan to be submitted

42. Details of proposed footbridge over the Beck to link the school and playing pitch shall be submitted

1.0 INTRODUCTION

- 1.1 The application is for an outline application for residential development and land reserved for a primary school with the principle of development and access applied for and all other matters reserved. The scheme is brought to Panel due to the scale of development, number of objections from local residents and the fact that Panel refused an application for development on this site in 2014.

2.0 PROPOSAL:

- 2.1 The application is an outline application for residential development for up to 100 dwellings and land reserved for a primary school with the principle of development and means of access applied for and all other matters reserved. The main access for the proposal will be on Otley Road to the north of the site opposite the entrance to Kingsley Avenue. This access will serve the majority of the site along with the land for the proposed two form entry primary school. There will be a second access to the south of the site which will serve the residential development to the south of the school. This access will be off Ash Road through the existing development known as Centurion Fields which was constructed after an appeal in 2011.
- 2.2 An indicative masterplan has been submitted to give an indication on how the school and housing could be accommodated on the site. The layout shows 94 dwellings with the school shown in the centre of the site with a loop of housing to the north and two cul de sacs of housing to the south with no through route. Whilst this masterplan at the moment is indicative it is anticipated that at reserved matters stage a layout would be submitted which shows all off the site accessed via Otley Road and the access to Ash Road (Centurion Fields) closed when the development is completed. All the proposed built development is to the west of the Beck. To the east of the Beck there will be two attenuation ponds for the drainage which will be landscaped and significant tree planting. The proposal also includes a piece of land for a playing field on the eastern side of the Beck connected to the school. This will be a grassed playing pitch with no floodlighting and fencing and will be low key. A footbridge will be required to link the school to the playing pitch but the precise location for this is not known at this stage.
- 2.3 The proposed access for the site will be off Otley Road and will take the form of a priority junction which will involve carriageway widening, the relocation of existing bus stops and the extension of the 30mph speed limit further to the north along Otley Road. Other highway works proposed by the development will be
- the extension of the eastern footway on Otley Road from the site access to the existing footway at Holt Avenue.
 - the provision of a formal signalised crossing on Otley Road in the vicinity of footpath 17.
 - the upgrading of this public footpath 17 to an all-weather surface with a regrad to meet accessibility requirements. This footpath is from Church Lane to Otley Road.
 - Traffic calming on Church Lane and a financial contribution of £100,000 to improvements to the Church Lane/Otley Road/Farrar Lane junction.

3.0 SITE AND SURROUNDINGS:

- 3.1 The site is currently open fields located to the east of Otley Road and sandwiched between Otley Road and Church Lane. The land slopes down from Otley Road towards the Beck which is situated in the middle of the fields between Otley Road and Church Lane. The land then slopes back up to Church Lane although the fields which form a boundary with Church Lane are not included in the application site. There are a small number of houses to the west of the site off Otley Road in an area known as The Willows and the back gardens for these properties have their boundary with the application site. To the south of this application site is a recently constructed residential development known as Centurion Fields and beyond this the main urban area of Adel. On the other side of Otley Road are further residential properties. This side also includes a public house and a small parade of shops including a small supermarket. To the north of the site are open fields which are located in green belt. On the other side of Church Lane is a grade 1 listed church known as St John the Baptist's Church. This church is one of the finest examples of twelfth-century church buildings in the country. The setting of this church and associated conservation area retain a strong rural character and this enables an appreciation of the early origins and historically isolated position and therefore makes a positive contribution to the significance of the heritage assets. The site is outside of the Conservation Area with the boundary of the Conservation Area being Church Lane itself. Some of the trees on the site are covered by a Tree Preservation Order mainly the groups of trees which forms the boundaries on the site.

4.0 RELEVANT PLANNING HISTORY:

14/01660/OT – outline application for residential development was refused on 9th October 2014 after a City Plans Panel decision on the same day. The application was refused for the following reasons

1. The site would be premature and contrary to policy N34 of the UDP and fails to meet the interim housing delivery policy
2. The applicant has failed to demonstrate that the proposals can be accommodated safely and satisfactory on the local highway network in relation to the impact on the proposed NGT junction designs
3. The applicant has failed to demonstrate that the proposals can be accommodated safely and satisfactory on the local highway network
4. The proposed signalised junction on the A660 will delay movements and increase accidents on the A660.
5. The absence of a signed s106 agreement.

5.0 HISTORY OF NEGOTIATIONS

- 5.1 The scheme has been amended during the processing of the application mainly in relation to the site access, off site highway works and location of the proposed school playgrounds.
- 5.2 Originally the site access proposed off Otley Road was traffic lights and this has been amended to a priority junction. Officers have negotiated the required off site highway works which will be obtained via a section 106 agreement and conditions. Finally the proposed school playgrounds were proposed on the eastern side of the Beck and these have been moved to the western side.

6.0 PUBLIC/LOCAL RESPONSE:

- 6.1 The application was advertised by a major site notice which was erected on 28th October 2016 and expired on 18th November 2016. The application was also advertised in the Yorkshire Evening Post on 2 November 2016 which expired on 25th November 2016

Councillors Barry and Caroline Anderson have objected to the scheme on the following grounds:

- Contrary to Adel Neighbourhood Design Statement
- Contrary to draft Adel Neighbourhood Plan
- We support the objection from Adel Neighbourhood Forum
- Impact on conservation area
- Impact on listed church Adel St John the Baptist and its historical significance
- Proposed footpath pitch with fencing and floodlighting would impact on listed church
- Adel has no need for further housing
- The SAP has this site with a smaller capacity
- Other permissions in Adel with no infrastructure installed
- Downward trend in population projections so housing number requirements should be reviewed
- The site was refused permission in November 2014
- Premature in advance of the SAP
- Outer North West Housing Market Characteristic Area housing needs met without the need for this site
- Takes a small part of greenbelt
- Drainage concerns with large attenuation pools needed to be installed
- Access to school through housing which is not acceptable to future residents
- Impact off school on traffic flows
- Impact off school in terms of parking on surrounding roads
- No room on plans for school playground area
- Surrounding roads cannot accommodate development
- Rat running on the existing surrounding streets
- Current bus services are inadequate to accommodate additional houses
- Pedestrian accesses around the site are not clear
- Number of infrastructure deficits that need to be put in place need to be considered
- Houses sizes are not the house sizes that Adel needs. There is a need for smaller houses for older residents
- Proper debate needs to take place regarding affordable housing
- No consultation by the developers with local residents
- Brownfields site should be developed first
- Consideration of local infrastructure such as shopping

Greg Mulholland MP has objected for the following reasons-

- The use of greenfield land when brownfield site should have first priority
- Impact on the setting of Grade 1 listed church of Adel Parish Church of St John the Baptist
- Premature in advance of Site Allocations Plan and Adel Neighbourhood Plan
- Impact on surrounding infrastructure, PROW, schools and A660 corridor and surrounding highway junctions
-

Adel Neighbourhood Forum have also objected to the scheme for the following reasons

- The site is a PAS site so policy N34 is applicable
- The site is not sustainable for the following reasons

Highways

- Increase traffic on the A660 on top of other developments approved nearby
- Traffic lights for main junction will delay traffic on A660 and cause rat running through existing estates
- Additional traffic on Church Lane which is already at capacity
- The development will encourage the use of cars to local facilities

School

- Existing schools are already at capacity
- School will generate traffic through existing estates
- School has limited playground space allocated
- concerns fencing and floodlights will be linked with school and impact on church

Housing Mix

- Adel needs wider mix of housing sizes
- concern 'shared ownership' properties will not be available to wide proportion of the population including local people who wish to downsize
- 2.5 storey houses will adversely impact on the views from the church and conservation area

Conservation and design

- proposal pays scant regard to the design opportunities offered by the Adel location and no level of innovation.
- no positive design quality
- poor design and should be refused

Green and open space

- if developed this green open space cannot be replaced

Consultation

- level of consultation poor and residents not given opportunity for commenting on the proposal before the application was submitted

Public transport

- The main X84 will also serve the development at Bramhope and Otley so as a result residents will use the No 1.

Prematurity

Development is premature as Leeds Core Strategy has not yet been determined and concerns regarding capacity of local schools and local GPs to absorb the increased demand

Council still needs to determine its transport strategy following rejection of NGT development

Site of setting of church is not preserved and setting of the conservation area would be substantially harmed.

Proposals might be economically sustainable but not socially and environmentally sustainable.

125 letters of objection from 118 dwellings have been received on the following grounds:

- Local infrastructure cannot cope with the increase in traffic
- Ash Road is unsuitable for further traffic to both the new houses and the school
- Parking issues for existing residents
- Junction of Church Avenue and Church Lane cannot cope with more cars

- Proposed new access cannot cope with the increase in traffic
- Church Lane/Otley Road/Farrar Lane junction already queues
- Why are more houses needed as building already in the area
- Previous application rejected and so should this one
- Overdevelopment of the green belt
- Out of character with the environment
- Impact on the historical grade 1 listed St Johns church
- Impact on the conservation area
- Impact on the public footpaths
- No guarantee that the new primary school will go ahead
- Impact on the character of Adel
- Premature ahead of approval of the Site Allocations Plan
- Premature ahead of approval of Adel Neighbourhood Plan
- Impact on views from Adel Dam Nature Reserve
- Impact on area of archaeological significance
- Dwelling style design is out of keeping with Adel's strong character
- Development on a greenfield site whilst brownfield sites still available
- Impact on trees protected with a Tree Preservation Order and removal
- Loss of trees and impact on area
- A full environmental assessment has not been submitted
- Impact on wildlife and ecology
- Risk of flooding
- There should be no connection through the site to prevent rat running
- How will school traffic/parking be managed
- Impact off construction traffic on highway network
- Non-compliance with the NPPF
- Impact on the buffer zone at the edge of Centurion Fields
- Dispersal of traffic through Gainsborough/Kingsley Drive development
- Too many larger houses and not enough smaller homes
- Lack of public consultation
- Removal of bus stop and impact on the elderly
- Impact on visual amenity
- Tranquillity and peacefulness of this location will be lost
- Increase in pollution and reduction in air quality due to increased traffic fumes
- No infrastructure such as doctors, shops
- Adel phone exchange cannot handle additional demands for broadband.
- Loss of good quality agricultural land
- Insufficient land to accommodate school, playground and number of houses proposed
- Transport assessment provided contains inaccuracies and assumptions which cannot be regarded as being 'robust'
- Alternatives to provide additional primary school capacity should be explored
- Two bed smaller houses are needed not 4/5 bedroomed houses
- Area for school looks too small
- Impact on emergency service access to Centurion Fields and Holt Avenue
- Impact on Golden Acre Park
- None compliance with NPPF
- School not needed as primary school in Cookridge are not full
- No information who is to pay and build the proposed school
- Highway data is incomplete and error bound with the sample duration being selective and statistically insignificant
- Is the primary school a 1FE or 2FE as a 2FE will have a greater cumulative impact on site access and vehicle trips
- A 2FE school would be an oversupply of primary school places specifically for the Adel area

- What is the catchment area for the proposed school
- Impact of footpath 'Corpse Way' being tarmac and loss of stone stiles and steps
- Playing areas for the school should not be to the east of the Beck

7.0 CONSULTATION RESPONSES:

West Yorkshire Combined Authority require £20,000 for upgrade of one bus stop and a real time display for another and a metrocard contribution of £48,125

Highways – No highway objection subject to provision of the s106 agreement and condition. The s106 agreement should cover

- Financial contribution towards improvement works at the Church Lane/Farrer Lane/Otley Road junction of £100,000 prior to the occupation of the 50th dwelling
- Financial contribution of £6,000 to Traffic Orders on the Kingsley/Gainsborough estate should the signalised site access junction be constructed
- Sustainable Travel Fund of £481.25 per unit
- Travel Plan Monitoring fee
- Bus stop contribution

Historic England – Previously advised that no development should take place east of the Beck in order to protect the setting of the Grade 1 listed church and Adel Conservation Area. This layout is welcomed and we do not object to the application but recommend the less than substantial harm the proposals would cause should be weighed against the public benefits of the scheme in accordance with paragraph 134 of the NPPF. Historic England did object to a layout that showed the hard surfaced play areas on the eastern side of the Beck.

Flood risk management – Approval subject to conditions

PROW – the proposed joint footpath and cycle route width and surface needs to be improved to adoptable standards, a safe crossing point on the estate access road also needs to be provided

Travelwise – Request s106 agreement for travel plan review fee (£2,500) and residential travel plan fund (£48,125)

West Yorkshire Archaeology – recommend an archaeological evaluation is carried out preferably before permission is granted or as a condition

Natural England – No detailed comments to make but need to take on board legislation in relation to impact on the natural environment

8.0 PLANNING POLICIES:

Development Plan

8.1 Section 38 of the Planning and Compulsory Purchase Act 2004 requires that planning applications are determined in accordance with the development plan unless material considerations indicate otherwise. The Development Plan for Leeds Comprises the Adopted Core Strategy (November 2014), saved policies within the Leeds Unitary Development Plan (Review 2006) and the Natural Resources and Waste Development Plan Document (2013).

8.2 In terms of section 66 (1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 it states that in considering whether to grant planning permission for

development which affects a listed building or its setting the Local Planning Authority shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historical interest which it processes.

UDP designation/Emerging Site Allocations Plan

- 8.3 The application site is designated as a Protected Area of Search in the UDP. Within the draft Site Allocations Plan (reference HG2-18) it is allocated for housing within phase 2 with an indicative capacity of 87 units and a primary school under policy HG2. The original allocation in the SAP was for 58 units but this has been updated after a more detailed analysis of the site was undertaken. The site is located within the Outer North West Characteristic Area which should have 2000 dwellings throughout the plan period. Within the SAP it is stated that a satisfactory access from the A660 via a staggered junction is required. A landscaped buffer is required to the Beck on ecological grounds. The isolated setting of the Grade 1 listed St Johns Church contributes to its significance so a substantial buffer is required to preserve its importance, and the site is within the setting of the Conservation Area so any development should preserve or enhance the character and appearance of the Conservation Area. Finally part of the site should be retained for the provision of a school.

Adopted Core Strategy

- 8.4 The Core Strategy is the development plan for the whole of the Leeds district. The following core strategy policies are considered most relevant

Spatial Policy 1 – The location of development
Spatial Policy 6 – Housing requirement and the allocation of housing land
Spatial Policy 7 – Distribution of housing land and allocations
Policy H1 – Managed release of housing sites
Policy H2 – Housing on non-allocated sites
Policy H3 – Density of residential development
Policy H4 – Housing mix
Policy H5 – Affordable housing
Policy P10 - Design
Policy P11 – Listed buildings and conservation
Policy P12 – Landscape
Policy T1 – Transport management
Policy T2 - Accessibility requirements and new development
Policy G4 – New green space provision
Policy G8 – Protection of important species and habitats
Policy ID2 – Planning obligations and developer contributions.
Policy EN2 – Sustainable design and construction
Policy EN5 – Managing flood risk

Saved Policies - Leeds UDP (2006)

- 8.5 The following saved policies within the UDP are considered most relevant to the determination of this application:

Policy GP5 - Development Proposals should resolve detailed planning considerations.
Policy T7A – Cycle parking guidelines
Policy T7B – Motor cycle parking
Policy BD2 – Design and siting of new buildings

Policy BD5- Amenity and new buildings
Policy LD1 – Landscaping schemes
N23, N24 and N25 – Landscape design and boundary treatment

Relevant supplementary guidance:

- 8.6 Supplementary Planning Guidance provides a more detailed explanation of how strategic policies of the Unitary Development Plan can be practically implemented. The following SPGs are relevant and have been included in the Local Development Scheme, with the intention to retain these documents as 'guidance' for local planning purposes:

Neighbourhoods for Living SPG
Affordable housing
Designing for community safety – a residential guide
Public Transport Improvements and Developer Contributions
Street Design Guide
Adel St John's Conservation Area
Guideline Distances – Development to Trees
Draft Adel Neighbourhood Design
Draft Adel Neighbourhood Plan

National Planning Policy Framework (NPPF)

- 8.7 The National Planning Policy Framework (NPPF), published on 27th March 2012, and the National Planning Practice Guidance (NPPG), published March 2014 replaces previous Planning Policy Guidance/Statements in setting out the Government's planning policies for England and how these are expected to be applied. One of the key principles at the heart of the Framework is a presumption in favour of Sustainable Development.
- 8.8 The NPPF constitutes guidance for Local Planning Authorities and its introduction has not changed the legal requirement that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise.
- 8.9 The NPPF establishes at Paragraph 7 that there are three dimensions to sustainable development: economic, social and environmental of which the provision of a strong, vibrant and healthy community by providing the supply of housing required to meet the needs of present and future generations is identified as a key aspect of the social role. Within the economic role, it is also acknowledged that a strong and competitive economy can be achieved by ensuring that sufficient land of the right type is available in the right places and at the right time to support growth and innovation.
- 8.10 Paragraph 17 sets out twelve core planning principles, including to proactively drive and support sustainable economic development to deliver the homes, business and industrial units, infrastructure and thriving local places that the country needs, ensuring high quality design but also encouraging the effective use of land by reusing land that has been previously developed (brownfield land), provided that it is not of high environmental value.
- 8.11 With specific regard to housing applications, the NPPF states at paragraph 47 that to boost the supply of housing, local planning authorities must identify and update

annually a supply of specific deliverable sites sufficient to provide five years' worth of housing against their housing requirements with an additional of 5% (moved forward from later in the plan period) to ensure choice and competition in the market of land. Deliverable sites should be available now, be in a suitable location and be achievable with a realistic prospect that housing will be delivered on the site within 5 years. It states that where there has been a record of persistent under delivery of housing, local planning authorities should increase the buffer to 20%.

8.12 Paragraph 49 of the National Planning Policy Framework states the following:

"Housing applications should be considered in the context of the presumption in favour of sustainable development. Relevant policies for the supply of housing should not be considered up-to-date if the local planning authority cannot demonstrate a five-year supply of deliverable housing sites."

8.13 In the appeal decision dated 8th June 2016 in relation to land at Grove Road, Boston Spa in accordance with APP/N4720/A/13/2208551, the Secretary of State took the view that on the basis of the evidence available to him at that time, the Council was unable to demonstrate a deliverable 5-year supply of housing land. This conclusion has subsequently been reinforced by the Secretary of State decision(s) on the conjoined appeals at Breary Lane, Bramhope, Bradford Road, East Ardsley and Leeds Road, Collingham (the "Ken Barton Conjoined Appeals"), which were considered by Planning Inspector Ken Barton in Spring 2016. On 22nd December 2016, the Secretary of State issued his decision on these conjoined appeals and agreed with the Inspector's conclusions that the appeals should be allowed. In reaching the decisions on the three appeals the Secretary of State concluded: The buffer for Leeds City Council must be 20% - so that the 5 year housing land supply requirement across the City could be 31,898 or 6379 units per annum.

The Secretary of State agrees with the Inspector's conclusion that the failure [of Leeds City Council] to produce an Adopted SAP (Site Allocations Plan) until at least December 2017 means that there is no policy set out to show how delivery of any houses, never mind the magnitude required, will actually take place; that the safety margin of 2262 dwellings can soon be whittled away when realism is applied and that the Council has failed to demonstrate a robust 5 year housing land supply. The Secretary of State therefore agrees with the Inspector's conclusion that the solution is to deliver housing now, including much needed affordable housing.

Having regard to the Development Plan position, the Secretary of State agrees with the Inspector that there is no 5-year housing land supply. Therefore, whilst he agrees with the Inspector that the UDPR policy N34, which designates sites as a Protected Area of Search (PAS) is a policy for the supply of housing, he also agrees with the Inspectors conclusion that policy N34 cannot be considered up-to-date. He further agrees with the Inspector that, rather than being a restrictive policy, the purpose of Policy N34 was to safeguard land to meet longer term development needs, so that, as it envisages development, the appropriate test to apply is whether any adverse impacts of granting permission would significantly and demonstrably outweigh the benefits when assessed against the policies in the Framework as a whole.

8.14 Accordingly, the Council is now in the position that it does not have a 5 year housing supply and the policies within the Unitary Development Plan and Core Strategy that are relevant to the supply of housing are considered to be out of date. Paragraph 14 of the NPPF is, therefore, now particularly relevant, which states the following:

“At the heart of the National Planning Policy Framework is a presumption in favour of sustainable development, which should be seen as a golden thread running through both plan-making and decision-taking.

For decision-taking this means:

Approving development proposals that accord with the development plan without delay; and where the development plan is absent, silent or relevant policies are out-of-date, granting permission unless:

— any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole; or

— Specific policies in this Framework indicate development should be restricted.”

It is important to note that an ‘out of date’ policy does not become irrelevant and it is therefore the case that an assessment must be made in respect of the weight to be attached to such policies in the planning balance of decision making overall.

- 8.15 In relation to highway matters, Paragraph 32 of the NPPF states that all developments that generate significant amounts of movement should be supported by a Transport Statement or Transport Assessment. Plans and decisions should take account of whether: the opportunities for sustainable transport modes have been taken up depending on the nature and location of the site, to reduce the need for major transport infrastructure safe and suitable access to the site can be achieved for all people; and improvements can be undertaken within the transport network that cost effectively limit the significant impacts of the development. Development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe.
- 8.16 Finally, also of relevance to this application is guidance within the NPPF in relation to policy implementation and the status to be given to emerging plans. Paragraph 216 of the NPPF advises decision-takers may also give weight to relevant policies in emerging plans according to:
- (i) The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
 - (ii) The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
 - (iii) The degree of consistency of the relevant policies in the emerging plan to the policies in this Framework (the closer the policies in the emerging plan to the policies in the Framework, the greater the weight that may be given).

This is pertinent to the site allocation process in Leeds.

Housing White Paper

- 8.17 This was a draft publication for consultation published on 7th February 2017 so it carries little weight.

9.0 MAIN ISSUES

1. Principle of development
2. Highway matters
3. Impact on the church and conservation area
4. Proposed school
5. Affordable housing
6. Greenspace
7. Residential amenity
8. Trees
9. Ecology
10. Drainage
11. Archaeological significance
12. Previous refusal
13. Representations
14. CIL

10.0 APPRAISAL

Principle of development

- 10.1 In late December 2016 the Council received three appeal decisions related to residential development at Breary Lane East, Bramhope; Leeds Road, Collingham; and Bradford Road, East Ardsley. The appeals were allowed and it was concluded by the Inspector that Leeds is presently unable to demonstrate a 5 year housing land supply. (5YHLS). As a result, at the current time, in accordance with paragraph 49 of the National Planning Policy Framework (NPPF), policies within the Unitary Development Plan (UDP), Core Strategy and Natural Resources and Waste DPD that are relevant to the supply of houses are not considered to be up to date and therefore housing applications will be considered in the context of the presumption in favour of sustainable development.
- 10.2 The application site is part of a larger site designated within the UDP as a Protected Area of Search. (PAS). It is part of a pool of land which was considered to offer potential to meet longer term development needs. Policy N34 is a policy for the supply of houses and as there is no 5 year land supply in Leeds, this policy cannot be considered to be up to date and paragraph 14 of the NPPF becomes applicable. The Inspector has found that rather than being a restrictive policy at paragraph 85 of the NPPF, bullet points 3 and 4, specifically relate to safeguarded land, which whilst not allocated at present time, meets the longer term development needs. The test that then applies is whether any adverse impacts of granting planning permission significantly and demonstrably outweigh the benefits, when assessed against the policies in the NPPF as a whole. The conclusion of this test will be a material consideration to be weighed in the balance when considering whether material considerations exist to outweigh the presumption in favour of the development plan in accordance with section 38(6).
- 10.3 A thorough review of all UDP PAS sites has been undertaken as part of the preparation of the Site Allocations Plan (SAP). As a result a comprehensive site assessment exercise, this PAS site is identified for allocation for housing in the Site Allocations Plan (reference HG2-18) with a proposed capacity of 87 units. The site is considered to be an acceptable allocation in the SAP as it is a relatively sustainable

location for new housing development on the edge of the existing urban area. The site is deliverable now and provides land for a school which is required in the area. The site is proposed for release as a Phase 2 (Policy H1 of the Core Strategy) as it is a Greenfield allocation and is an extension to the main urban area rather than sites which are within the main urban area which are allocated as phase 1. In light of the absence of a 5YHLS and on the basis of the proposed SAP allocation, it is considered that on balance, the site provides a sustainable location for housing, when assessed against the Core Strategy Policies and NPPF as a whole, subject to the specific and detailed site considerations which are discussed in the rest of this report. The capacity of housing units identified in the SAP (87 units) so the application for up to 100 dwellings reflects this capacity within the SAP and is considered acceptable.

- 10.4 Therefore the principle of development for residential development on this site is considered to be acceptable.

Highways

- 10.5 The site is identified in the draft SAP and the highway requirement in that document is for 87 units with part of the site being required for a primary school. The masterplan submitted shows a scheme of 98 houses and land reserved for a primary school. Within the SAP there is a highway requirement that satisfactory access arrangements from the site are to be agreed along with traffic management measures on Church Lane and off site highway improvements to the A660
- 10.6 The Site Allocations Transport Background Paper that was considered by Development Plans Panel on 10th January identifies that the A6120/A660 Lawnswood is a congestion hotspot requiring intervention to improve bus journey times. This site along with the larger Breary Lane, Bramhope site HG2-17 is identified as having a cumulative impact on the junction. The development impact is 42 two –way trips in the morning peak and 33 trips in the evening peak. The development will also impact to a lesser degree on the A660 corridor into the city centre where three further hotspot junctions have been identified. However, rather than improvements to these further afield hotspots it is considered that the improvements offered by this development in the closer proximity to the site in this instance are preferred.

Impact on surrounding network:

- 10.7 The development has been assessed in terms of the impact of the generation of traffic on the major junctions in the area. The assessment includes the impact of the traffic from a potential 2FE primary school at the Otley Road site access and the expected growth in traffic resulting from the SAP as well as the proposed housing on this site. There are two areas which are in close proximity to the site which will be most affected by the proposal which are the Church Lane/Farrar Lane/Otley Road junction and Church Lane itself.

i) Church Lane/Farrar Lane/Otley Road

- 10.8 The development is predicted to contribute 38 two way trips in the morning peak and 30 two way trips in the evening peak to the traffic movement at this junction. A comparison of the level of development traffic with the total expected increase in traffic from 2015 as a result of growth shows the development would contribute to 12% of the morning south bound increase and 6% of the northbound increase,

similarly in the evening the development contributes 8% to the southbound and 7% to the northbound increases. The results of the modelling identified that the combination of this development along with traffic growth from other development will have a severe impact on this junction with the morning peak having close to or above 100% saturation (Otley Road (N) 97.5% and Church Lane 101.0%). Additionally there is a significant local pressure to provide pedestrian crossing facilities and still maintain capacity. The Councils UTMC team have suggested a way of improving the junction to provide pedestrian crossing facilities and still maintain capacity. It is considered appropriate that the development site should contribute towards the cost of these works, the scheme is likely to incur substantial cost, a contribution of £100,000 from this development has been agreed and will be included within the S106 agreement. As it will take several years for the predicted growth to materialise, it is agreed that the payment can be made on the occupation of the 50th dwelling.

ii) Church Lane

- 10.9 The development will generate pedestrian and vehicle movement across and along Church Lane, traffic calming is proposed north of Adel Lane that will compliment that provided by the Boddington and DWP sites to the south. Important features include a raised pedestrian crossing where the public footpath crosses to the church, there is a route along footpath 60 from near Holt Close to Long Causeway that is used for school and leisure purposes, as this is also a vehicular route to a number of properties, it is not suitable to place a raised pedestrian table at its mouth, although features could be placed to slow traffic and highlight the crossing point. The final details can be considered at reserved matters stage.
- 10.10 It is considered that there would be no benefit to altering the Adel Lane / Church Lane priority at this junction as it would be more likely to delay traffic on Adel Lane than the current arrangement. It also has the potential to cause a road safety problem.
- 10.11 In terms of other junctions within the vicinity of the site it is considered that the impact on these junctions is minimal due to the low level of traffic reaching them from the development.

Site access:

- 10.12 There were two options for the site access being either in the form of a traffic light controlled cross roads or a priority junction with ghost island turning lanes and a slight stagger between the opposing arms of the site access and Kingsley Drive. The original masterplan and preferred option by the developer was for the traffic lights, with the masterplan in front of you today showing the ghost islands option.
- 10.13 Both junction options have been shown to operate satisfactorily from a traffic capacity point of view, each has merits and potential problems and need to be considered with the potential of a future school. Both options involve widening the carriageway in the vicinity of the junction to provide space for right turning vehicles to wait without blocking through traffic.
- 10.14 Both options require the relocation of bus stops, whilst the ideal location for a northbound stop would be to the south of Kingsley Drive, WYCA's view is that this would be unpopular with whichever residents were to have the stop outside of their property and that it would be better to relocate the stop, currently on the north western corner of Kingsley Drive further north. The possibility of an additional stop

with a pole to the south of the junction can be included and delivered if possible. The southbound stop can move further south without directly impacting on residential frontage. The relocated stop is likely to attract residents from the southern section of the site via the public right of way as it is the shortest route to the stop, the verge from the footpath to the stop will be converted to footway and public right of way improved in both surface material and graded to remove the steps up to the A660. Precise details in relation to this matter can be submitted with any reserve matters application.

- 10.15 In terms of the option of the signalised junction this would have the advantage of providing formal pedestrian crossing facilities across Otley Road, this would give a safe link from the northbound bus stop to the site and benefit existing users of the southern stop. A formal crossing at the northern end of the built up area would help integrate the two side of Otley Road.
- 10.16 The signalised junction should serve the totality of the development once complete. This would not increase residential development traffic on Holt Avenue and would encourage more future school drop off traffic into the site and less via Holt Avenue. As a result it may reduce the impact on the Church Lane/Adel Lane congestion issues. It is understood that the development may be built from both Holt Avenue and Otley Road, however a vehicular closure at Holt Avenue could be inserted at the time that the two halves of development join within the site.
- 10.17 The disadvantage of a signalised junction is that it would have a detrimental impact on traffic along the A660. It would cause delay and stopping and starting to traffic on Otley Road especially at off peak times when there is little other delay and turning into the side roads would not be problematic.
- 10.18 Residents on Kingsley Drive estate which would be served by this signalised junction have objected as they consider that there would be an increase in traffic through their residential area as it would provide an easier way in and out of the estate and that parents would park there to take their children to the primary school as it would be quicker to walk using the pedestrian crossing than waiting at the traffic lights.
- 10.19 The alternative junction which is for consideration today is the ghost island arrangement with non-hooking right turns into the side roads by means of a slight offset between the side roads.
- 10.20 The modelling of the junction shows that the junction will operate without significant queuing during the peak periods. Furthermore during the off peak periods, through traffic on Otley Road would not be delayed. There would also be no added advantage to drivers wishing to head north from the west of Otley Road cutting through the existing residential area.
- 10.21 The disadvantage of this option is that there are no formal pedestrian crossing facilities provided. The development will generate a demand for pedestrians to cross Otley Road safely, and it would be feasible to locate a crossing point in the vicinity of where the public right of way emerges onto Otley Road and the footpath from Gainsborough Avenue emerges. At this stage it should be assumed that a signalised controlled solution will be required. As with the signalised junction it would be preferred if all the development was eventually served from the Otley Road access.

- 10.22 Having assessed both the junction options on balance it is considered that the junction arrangement is preferred and this is shown on the masterplan presented to the meeting today.

Other off site highway improvements:

- 10.23 The development will urbanise the eastern side of Otley Road, to improve drivers perceptions that this is the case, the 30mph speed limit will need to be reinforced and moved further north. There is a need to provide a pedestrian link to bus stops and provide a variety of route choices for pedestrians with the existing eastern verge from the site access to the existing footway near Holt Avenue being reconstructed as footway with both junction scenarios. A number of trees, mostly self-sown varieties and some planting that has occurred on highway land without consent will need to be removed from the verge but these are low quality in terms of visual amenity value.
- 10.24 The public right of way which crosses the site should be upgraded to a surface usable in all-weather most likely to be tarmac with a gravel bound surface and the steps adjacent to Otley Road to remove the steps and at Church Lane end diverted around the old stone stile. As it will be a shared pedestrian and cycle track it will need to be 3m wide.

Accessibility:

- 10.25 The accessibility of the site has been improved by the now built Holt Avenue development that has increased pedestrian penetration of the site.
- 10.26 The site is reasonably accessible, whilst not fully meeting core strategy standards. The entire site is in 400m walk of the X84 bus stop, this is a 20 min frequency service, the walk distance can be improved by the provision of an additional section of footway on Otley Road as already described. The service No1 which is a high frequency service is within 400m walk of the southern third of the site. Primary and secondary schools are within the required walk distance and the additional primary school on the site will clearly be much closer.
- 10.27 There are a number of local facilities on the parade of shops on Otley Road, additionally the Co-Op which provides general grocery shopping is 950m from the centre of the site, a little over the 800m guidance.
- 10.28 On balance the accessibility of the site is reasonably good and a reason for refusal on this issue would not be reasonable as opportunity is taken to improve the 'walkable neighbourhood' as clearly reduced car use is to be encouraged it follows that there will be increased pedestrian movement.
- 10.29 A travel plan has been submitted and a supporting travel plan mitigation fund will be required to deliver ongoing initiatives to encourage sustainable travel. A Sustainable Travel Fund of £481.25 per unit has been agreed along with other measures to improve the pedestrian and cycle environment.

Internal layout:

- 10.30 The proposal is for access only and the masterplan that has been submitted is indicative only, however there are a number of matters that would need to be addressed in any reserved matters layout.

- 10.31 The layout shows the school and the majority of the residential development being accessed via Otley Road with a smaller number of houses being served off Holt Avenue. There is a cycling/pedestrian link between the two but no vehicular access. This is supported as it will deter rat-running through the site, however it would be preferable on completion for the whole of the development to be served off the Otley Road access as this would avoid conflict with the on street parking within Holt Avenue and encourage parents to use the new development to drop off children at school and this could be a matter that is addressed at the reserved matters stage.
- 10.32 The layout of the development should also accommodate the need for parents and provide enough space for on street car parking when dropping and collecting from school and to avoid overspill into the existing residential areas, plus the existing layout within Holt Avenue does not allow for parked cars and turning around in this area is difficult. There may also be demand for the school sports pitch to be used outside of school hours so the new development will need to accommodate on street car parking for this facility.
- 10.33 The current layout appears to be dense and the level of car parking provided does not look adequate and any reserved matters layout would need to include parking levels in line with policy requirements. All construction traffic will need to access this site via Otley Road and not access the site via Holt Avenue which is considered to be unsuitable for this use.
- 10.34 Overall it is considered that the proposal is acceptable in terms of impact on surrounding highway network, proposed access and accessibility subject to measures within the s106 agreement and conditions. The scheme therefore complies with policy T2 of the Core Strategy and paragraph 32 of the NPPF.

Impact on the church and conservation area

- 10.35 The original plans for the previous refused planning applications (14/01874/OT and 14/1660/OT) were subject to objections by Historic England (English Heritage at the time) due to the proposed development on the eastern side of the Beck and its impact on Adel Church (a grade 1 listed building) and Adel Conservation Area. The plans for this refused application were revised to not include any development to the eastern side of the Beck and the application did not include a reason for refusal based upon the impact on the church and the conservation area.
- 10.36 The current application has taken on board the amended plans on the previous application by refocusing the housing development to the west of the Beck. The original illustrative masterplan showed just a playing pitch on the eastern side of the Beck which Historic England raised no objection to subject to the playing pitch having no lighting or fencing. Officers raised concerns that the land to the west of the Beck allocated for a school did not have enough space to accommodate an outside hard surfaced playing area. A revised masterplan was submitted which showed a hard surfaced playing area to the east of the Beck. Historic England along with officers objected to this revision due to the impact on the church and conservation area. A revised masterplan has been submitted which now includes the hard surfaced play area back on the western side of the Beck and the reduction of houses by 2 to accommodate this.
- 10.37 On this basis Historic England have not objected to the proposed development and state that the development would cause minor harm to the conservation area and minor-moderate harm to the significance of the Church of St John. The design of the

proposed playing pitch has the potential to increase the level of harm and they would not support floodlighting or fencing around the proposed playing pitch. They conclude stating that the less than substantial harm the proposals would cause need to be weighed against the public benefits of the scheme in accordance with paragraph 134 of the NPPF.

- 10.38 The proposed development will introduce a fairly dense built form into the setting of the Grade 1 listed church and the conservation area. However, the location of the proposed housing, the intervening distance and the proposed landscaping would reduce this level of harm. The proposed development will also be seen with the backdrop of the residential development in Adel whilst the church is on a slight hill higher than the Beck in the middle of the application site so views of the church and conservation area will still be seen with open land surrounding it.
- 10.39 There are a number of public benefits for the development both for the area as a whole and the settlement of Adel. For the area as a whole it would supply housing in the absence of a 5 year land supply as the site is currently deliverable. It would also provide jobs during the construction period. In terms of Adel it will provide a school which is needed in the settlement, it would provide affordable housing for local residents, and the local businesses would be supported by employees during the construction period and residents afterwards. On this basis it is considered that the public benefits outweigh the harm of the proposal and the scheme therefore complies with paragraph 134 of the NPPF.

In terms of section 66 (1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 it is considered that the proposal does preserve the listed building and its setting and any features of special architectural or historical interest which it processes.

The proposal also complies with policy P12 of the Core Strategy.

Proposed School

- 10.40 Throughout the work undertaken as part of the SAP and in consultation with Children's Services, HG2-18 is identified as a site which should include land reserved for a school in order to address the need for additional school places on the site and the adjacent area. Children Services advised that the area of land should equate to a 2 FE primary school, which is equivalent to approximately 2ha of land. The illustrative masterplan indicates the proposed location of the school in the middle of the site, which Children Services have indicated is acceptable in principle. Whilst the scheme is outline only and the illustrative masterplan is indicative it is important to ensure that the masterplan showed that the land provided for the school can accommodate all the school buildings, parking, access and hard play areas without the need to transgress over the eastern part of the beck other than for the grassed playing pitch. Historic England have stated that there is no objection to the location of the grassed playing pitch on the eastern side of the Beck providing it is low key and does not have fencing and floodlight. There will also need to be access from the proposed school to the playing pitch over the existing Beck. At this stage is not known where the best location for the bridge would be and this can be resolved at reserved matters stage. If it could not be accommodated with a site capacity of up to 100 dwellings the number of houses would need to be reduced. The revised layout shows that the school can be accommodated with just under 100 houses so is considered acceptable.

Affordable housing

- 10.41 To reflect Policy H5 of the Core Strategy a provision of 35% affordable housing is required on this site and this can be achieved through the s106 agreement.

Greenspace provision

- 10.42 The proposed site layout makes well in excess provision for greenspace in line with Policy G4. This can also be achieved through the s106 agreement.

Residential amenity

- 10.43 The scheme that has been submitted is indicative only at this stage. The properties that will be most affected by the proposal are the existing houses on Centurion Fields and The Willows. Any reserved matters application will have to provide adequate distances from these properties to the new dwellings to comply with Neighbourhoods for Living. Likewise the layout will need to comply with the distances within Neighbourhoods for Living in terms of distances between properties and garden sizes.
- 10.44 The existing properties could be impacted by the additional noise and disturbance from the comings and goings of residents and parents visiting the school although this is limited to short periods during the working week. In terms of the Willows these will have the edge of development close to them with the main roads throughout the development are away from these properties. These properties are already located next to the main A660 so any additional noise and disturbance should not have a detrimental impact.
- 10.45 The properties on Centurion Fields will for a while have more traffic to a number of additional houses to the southern part of the site but this will not be a through route at this time. These additional houses should not increase traffic noise to levels which will have a detrimental impact. There could be disturbance by parents doing the school run but as mentioned in the highways section it is hoped at reserve matters stage to achieve a layout which will accommodate the parking for the school parents north of the proposed primary school and use the A660 entrance. Also a condition is being attached that only 36 houses will be accessed by this route and once the development is complete this access will be closed to vehicular traffic.

Overall it is considered that the scheme will not have a detrimental impact on residential amenity and complies with policy GP5 of the UDP.

Trees

- 10.46 As this is outline for the principle of development and access only this is not a matter to be dealt with at this stage. However, there are trees that cover the site and some of them are covered by a Tree Preservation Order especially at the edge of the development so the layout would have to accommodate their retention and adequate distances to roots and canopies.
- 10.47 It is accepted that there will be some tree loss to accommodate the access at the northern part of the site and these trees are covered by a Tree Preservation Order. Additionally planting on the site would be required to mitigate for this loss.

- 10.48 A landscaping buffer would also be required on the eastern side of the Beck which is shown on the masterplan and a wider buffer than shown on the masterplan would be required on the northern boundary with the green belt to comply with policy N24 of the UDP.
- 10.49 Overall it is considered that a residential development can be accommodated on the site without a detrimental impact on the existing trees on the site and adequate landscaping and comply with policy P11 of the Core Strategy.

Ecology

- 10.50 In terms of ecology there are no planning policy designations affecting the site and the aim of the application is to enhance the ecology of the area. The area of landscaping adjacent to the Beck needs to be managed for wildlife and a Biodiversity Enhancement and Management Plan would need to be submitted.

Drainage

- 10.51 A flood risk assessment has been submitted and there has been no technical objection to this. Full details in relation to drainage matters would be dealt with at the reserve matters stage.

Archaeological significance

- 10.52 A Roman fort and later civil settlement is located to the north beyond Adel Mill and the full extent of this is not known. Research has also shown that during the medieval period Adel lay in an area where settlement in dispersed farmsteads was the norm.
- 10.53 Although there are no known sites or heritage assets within the development site the surrounding landscape contains remains of national and regional significance and archaeological evaluation is necessary in order to make a balanced judgement on the impact of the application.

For this reason a condition regarding full archaeological recording needs to be attached.

Previous refusal

- 10.54 The previous application was refused for five reasons and these have now been addressed in this application as follows;
1. The site would be premature and contrary to policy N34 of the UDP and fails to meet the interim housing delivery policy. *The interim housing delivery policy is no longer a current policy and as detailed above policy N34 is now out of date so this reason for refusal is no longer relevant*
 2. The applicant has failed to demonstrate that the proposals can be accommodated safely and satisfactorily on the local highway network in relation to the impact on the proposed NGT junction designs. *The applicant has now been*

able to show how an access can be achieved and NGT is no longer relevant to this reason for refusal is no longer relevant

3. The applicant has failed to demonstrate to that the proposals can be accommodated safely and satisfactory on the local highway network. *The applicant has now shown that the development can be accommodated on the local highway network so this reason for refusal is no longer relevant.*
4. The proposed signalised junction on the A660 will delay movements and increase accidents on the A660. *The access is no longer a signalised junction so this reason for refusal is no longer relevant.*
5. The absence of an s106 agreement. *The applicant has agreed to the requested requirements that will form part of the s106 agreement so this reason for refusal is no longer relevant.*

Overall the previous reasons for refusal of this scheme have been overcome and the application is now considered to be acceptable.

Representations

10.55 Most of the matters raised in the representations have been addressed above except for the following:

- Contrary to Adel Neighbourhood Design Statement
The document was is not adopted so carries little weight. However, within this document the site is discussed on page 63 and it lists a number of issues that need to be taken into consideration if development comes forward on the site. These issues have been addressed in the report above
- Contrary to draft Adel Neighbourhood Plan
The is at draft stage and carries little weight but it does refer to the application site and states that any future development on this site should take account of the need to retain the eastern part of the site as open land which this scheme does.
- The site was refused permission in November 2014
The site was refused permission in 2014 but the policy situation has changed since this time as discussed in this report
- Takes a small part of greenbelt
The site does not include any land within the green belt.
- No consultation by the developers with local residents
The developer did carry out consultation with local residents and have submitted a statement of community involvement with the application. Leaflets were distributed to approximately 210 properties surrounding the site and residents were requested to submit any comments to the agent.
- Brownfield sites should be developed first
The Inspector in relation to his recent appeal decisions on the three housing sites states that sites should be brought forward for development at this time to meet the demand required for housing in the area.
- Existing schools are already at capacity
This site provides a school to deal with the additional school places required
- No guarantee that the new primary school will go ahead

The school is a requirement within the SAP as it is considered that there is a need for a school in the area. The section 106 agreement will state that the land has to be reserved for a school until Leeds requires the land.

- No infrastructure such as doctors, shops
These are matters that will be controlled by the market and if the demand for the services is there then the market will respond with increasing the services. It is not a matter to refuse planning permission for.
- Adel phone exchange cannot handle additional demands for broadband.
This is not considered to be a planning matter and a refusal on this ground could not be justified.
- Loss of good quality agricultural land
The land is classified as class 3 agricultural lands which is good to moderate and is adjacent to the urban area which reduces its quality. Impact on emergency service access to Centurion Fields and Holt Avenue

Community Infrastructure Levy

- 10.56 The Community Infrastructure Levy (CIL) Charging Schedule was adopted on 12th November 2014 with the charges implemented from 6th April 2015 such that this application is CIL liable on commencement of development at a rate of £90 per square metre of chargeable floorspace. Due to the outline nature of this application, the floorspace is unknown at this stage. In any event, consideration of where any Strategic Fund CIL money is spent rests with Executive Board and will be decided with reference to the Regulation 123 list.

11.0 CONCLUSION

- 11.1 It is considered that the development of a residential scheme and land reserved for a school is acceptable in principle given the lack of a 5 year land supply and the fact that the site is a phase 2 in the Site Allocation Plan.
- 11.2 There will not be any harm in terms of highway safety, there will be no detrimental harm to residential amenity, and the impact on trees and ecology is considered acceptable.

Although there will be some less than substantial harm to the grade 1 Listed church it is considered that the following public benefits of the proposal:

- (a) Providing housing on a deliverable site in the absence of a five year land supply
- (b) Affordable housing
- (c) Employment
- (d) Providing a school
- (e) Support for local businesses

outweigh that harm.

- 11.3 As such the proposal is therefore recommended for approval, subject to conditions and a section 106 agreement.

Background Papers:

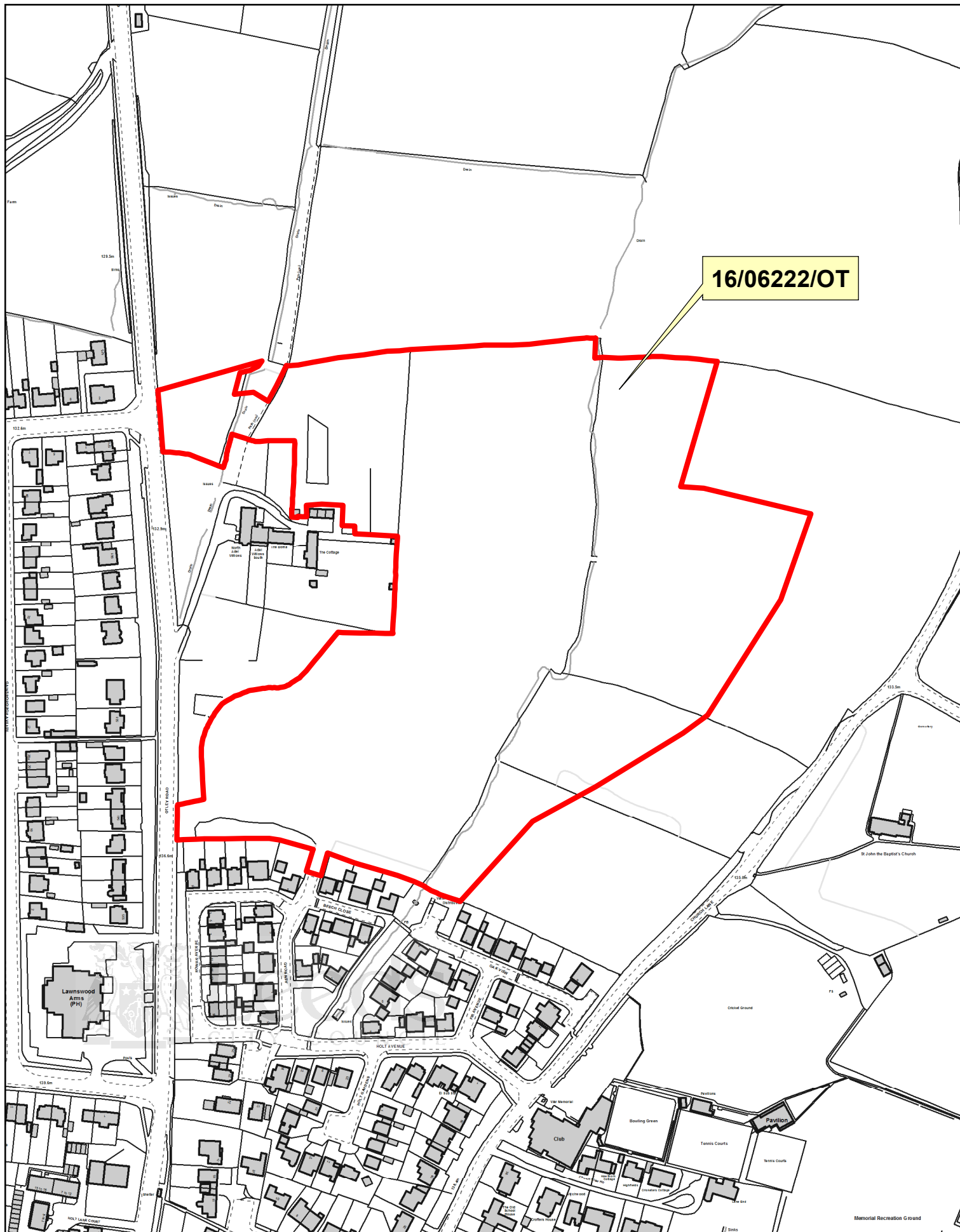
Certificate of ownership: signed by applicant.

Planning application file.16/06222/OT



- Key
- North
- Site boundary
 - Existing trees and hedgerow retained
 - Proposed front gardens
 - Proposed rear gardens
 - Proposed footpath / cycle route
 - Proposed native trees, buffer planting / woodland
 - Proposed native hedgerow
 - Proposed attenuation ponds: subject to engineers details
 - Proposed residential units
 - Proposed school
 - Proposed school play
 - Proposed private drives
 - Proposed primary road
 - Proposed natural LAP
 - Proposed bollards for emergency access only

Hallam Land
Land to the East of
Otley Road,
Adel
Scale: 1:2000 @ A3
Illustrative Masterplan



SOUTH AND WEST PLANS PANEL

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SCALE : 1/3000





Originator: Susie Watson

Tel: 0113 2224409

Report of the Chief Planning Officer

PLANS PANEL SOUTH & WEST

Date: 23rd March 2017

Subject: Application 16/07825/FU – Amendment of application 16/00869/FU for a single storey side extension (change a flat roof to a pitched roof) at 11 Church Crescent, Horsforth, LS18 5LF.

APPLICANT

Mrs Fengqin Chen

DATE VALID

3rd January 2017

TARGET DATE

28th February 2017

Electoral Wards Affected:

Horsforth

☐ Yes

Ward Members consulted
(referred to in report)

Specific Implications For:

Equality and Diversity ☐

Community Cohesion ☐

Narrowing the Gap ☐

RECOMMENDATION: GRANT PERMISSION subject to the specified conditions:

1. Standard time limit of 3 years to implement
2. Plans to be approved
3. Materials to match existing

1.0 INTRODUCTION

- 1.1 This application is reported to Plans Panel, due to a request from Councillor Cleasby who is concerned that the extension is not in compliance with the Neighbourhood Design Guide. This being a matter that gives rise to concerns in respect of impact on the streetscene and is an issue affecting more than neighbouring properties and it is therefore considered appropriate for referral to Plans Panel for determination.

2.0 PROPOSAL

- 2.1 This application follows on from an application approved, under delegated powers, in April 2016 for a single storey side extension to the property. The approved extension was authorized with a flat roof but it is now requested that consideration be given to adding a pitched roof to this. Work on the extension has commenced and the proposed pitched roof timbers are in place but are awaiting tiles.

3.0 SITE AND SURROUNDINGS

- 3.1 The application site contains a link-detached dwelling located in an established residential area. It is one of 7 originally identical properties stepping down the hill from north to south. The property is modern with a pitched roof and single storey attached element to the side incorporating a garage and entrance hall. Materials are beige bricks with hanging tile detail to the gable ends. The property can be accessed to both the front and rear - there is both pedestrian and vehicular highway access to the rear but only pedestrian access to the front. There are small front and rear gardens.

4.0 RELEVANT PLANNING HISTORY

- 4.1 16/00869/FU – single storey side extension – approved 1 April 2016.
- 4.2 16/9/00297/MOD – non-material amendment to replace flat roof with pitched roof – refused 19 December 2016 on the grounds that “the proposed amendment represents a material change to the planning permission granted and would require formal publication through the planning application process to allow interested parties to comment given it attaches to a neighbouring property. It therefore cannot be accepted through the non-material amendment process and instead requires the submission of a formal planning application.”
- 4.3 16/01184/UHD3 – A compliance check was made on the property following a complaint received on 30 November 2016 regarding the roof and height of the extension.

5.0 HISTORY OF NEGOTIATIONS

- 5.1 The application as originally submitted showed a roof with a lower ridge height than constructed on site. A revised plan to show the situation as built has therefore been submitted.

6.0 PUBLIC/LOCAL RESPONSE

- 6.1 The initial application was advertised by neighbour notification letters sent on the 5th of January 2017 with the publicity period expiring on the 30th of January 2017. In response to this publicity 2 letters were received from local residents (summarised below) and Horsforth Town Council commented that they neither support nor object to the application.

- A letter from the occupiers of 9 Church Crescent objects to the pitched roof on the grounds it is not in keeping with the original design of the property.
- A letter was received on behalf of the owners of 15 Church Crescent who contacted Councillor Cleasby about the proposal, asking the following questions /

making the following comments:

- What is the effect on our wall by the change in the roof?
- What will be the effect on our roof; will it lead to damp?
- The drawings are not accurate and show a roof lower than constructed.
- How will the floor of the proposed construction be supported?
- The changes have been made with no party wall agreement, no planning amendment and no agreement from us.

6.2 Upon the receipt of the revised plan to show the as-built situation, the neighbour at number 15 Church Crescent was re-notified. In response to this they have raised the following objections/issues:

- The properties will no longer look like a linked detached.
- Terracing is contrary to the Householder design Guide.
- All other link extensions have flat roofs.
- Sets a precedent.
- The bricks used do not match those existing and should be replaced.
- There is no updated party wall agreement in place.
- How will it affect number 15? E.g. will it result in damp issues, how will it affect the roof and walls, are there load bearing issues?

This neighbour has also submitted a further letter advising that they have had legal advice. They set out the history of what has happened (e.g. application for a flat roof, original resubmission plans inaccurate) and make a number of other comments which are summarised as follows:

- The approach of amending the application as work has and is progressing in line with differences to that initially conceived, has not allowed us or other the neighbours to properly consider or make representation on our concerns with the work that was proposed and is now taking or has taken place.
- Undue weight should not be given to any hardship caused to the applicant in having to remedy work carried out without planning permission.
- Previously set out concerns about visual amenity (terracing, no regard to local vernacular, non-matching materials).
- We are making separate enquires regarding the structural consequences of the work undertaken and proposed.

6.3 Councillor Brian Cleasby is concerned that the terracing effect is contrary to the Neighbourhood Design Guide.

7.0 CONSULTATIONS RESPONSES

7.1 None carried out due to the nature of the application.

8.0 PLANNING POLICIES

8.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that planning applications are determined in accordance with the development plan unless material considerations indicate otherwise.

Development Plan

8.2 The development plan for Leeds comprises of the adopted Core Strategy (November 2014), saved policies from the Leeds Unitary Development Plan (Review 2006) (UDP) and the Natural Resources and Waste Development Plan Document (January 2013).

8.3 The site is unallocated in the Development Plan.

8.4 The following Core Strategy policies are relevant:

P10 – High quality design

8.5 The following saved UDP policies are relevant:

GP5 – General planning considerations

BD6– Alterations and extensions to respect the original building

Householder Design Guide

8.6 The Householder Design Guide sets out guidance for extensions to dwellings within Leeds. Policies HDG1 and HDG2 are relevant and respectively relate to respecting the character of the main dwelling and the locality and protecting the amenity if neighbours.

HDG1 All alterations and extensions should respect the scale, form, proportions, character and appearance of the main dwelling and the locality. Particular attention should be paid to:

- i) the roof form and roof line;
- ii) window details;
- iii) architectural features;
- iv) boundary treatments and;
- v) materials.

Extensions or alterations which harm the character and appearance of the main dwelling or the locality will be resisted.

HDG2 All development proposals should protect the amenity of neighbours. Proposals which harm the existing residential amenity of neighbours through excessive overshadowing, overdominance or overlooking will be strongly resisted.

National Planning Policy

8.7 The National Planning Policy Framework (NPPF) sets out the Government's planning policies and contains policies on a range of issues including housing, sustainable development, green belt, conservation, the local economy and design. The NPPF must be taken into account in the preparation of local and neighbourhood plans and is a material consideration in planning decisions. Part 7 'requiring good design' is applicable to this proposal.

9.0 MAIN ISSUES

- Impact on Visual Amenity
- Impact on Residential Amenity
- Representations

10.0 APPRAISAL

Impact on visual amenity

- 10.1 It is considered that the proposed change from a flat roof to a pitched roof will, given its size, location, design and materials, be in keeping with the host dwelling and existing development in the locality. Its design (e.g. shape and form) is sympathetic to the design of the host dwelling and it is considered that due to its size it will remain subservient to the host property. Although it is readily visible from the public domain it does not appear as an overly dominant feature. The proposed materials will match those existing.
- 10.2 In light of the above it is therefore considered that the proposed works will not cause harm to the character and appearance of the application site or the existing street scene and that the proposal therefore complies with policies P10 of the Core Strategy, BD6 of the UDP and HDG1 of the Householder Design Guide.
- 10.3 It is noted that concerns have been expressed with regard to the pitched roof and how it is considered to be out of keeping with the property/locality. The properties in this row are detached houses which all have pitched roofs but are linked by single storey flat roof elements between them. It is therefore accepted that the addition of a pitched roof to this link element is different from the original character of single storey elements in this row of 7 properties. However, the main dwellings have pitched roofs as do other properties in the area, and pitched roof side extensions are not an uncommon feature in the wider locality. The adjoining property at 15 Church Crescent no longer has its single storey flat roof element to the side as this has been replaced with a large 2 storey, pitched roof side extension. The proposed pitched roof is relatively shallow and, given this pitch and that it slopes away from both the front and rear elevations, it does not form a large or dominant feature. It is also less dominant in the street scene than the existing extension at number 15. Overall, it is therefore considered that its scale and design is in keeping with the existing property / row and would not be harmful to its character.
- 10.4 Concerns have been expressed about the colour of bricks used for the outer walls of the extension as these don't match exactly those used on the original property. It is not known when the property was built but from examination of its design and character it is at least 40 years old, or thereabouts. An exact match for the bricks is therefore unlikely to be possible, as can also be seen in the extension at number 15 which is also constructed of bricks that don't exactly match the original property. In this case it is considered that the bricks used do tone with the original property and given the limited expanse of brickwork visible those used are not detrimental to visual amenity.

Impact on residential amenity

- 10.5 Given the nature of the proposal and its location in relation to neighbouring properties it is considered that it will have no impact on neighbouring living conditions in terms of overlooking, loss of light or dominance. As such the proposal complies with policy GP5 of the UDP and HDG2 of the Householder Design Guide, both of which aim to protect residential amenity.

Representations

- 10.7 It is considered that the planning issues raised by local residents have been addressed in the above appraisal. With regard to other issues raised by the neighbour at number 15 (see public/local response section) the following should be noted.
- 10.8 The case officer has written to the neighbour at number 15 to advise them that Planning Services does not get involved in technical matters of construction and we are therefore unable to answer questions relating to the impact on number 15 (e.g. does it make their wall load bearing, will it lead to damp, floor construction) as this is a matter covered by other legislation.
- 10.9 Whether or not the existing garage construction can support the new pitched roof is a matter to be addressed via Building Regulation Approval. The Council's Building Control section has advised that the Building Regulations Approval for this development is being dealt with by a company based in Newcastle and not the Council. They have also advised that the works being carried out are unlikely to raise any issues of concern for Building Regulations and that many of the issues raised by the neighbour at number 15 will need to be addressed privately under the Party Wall Act. Issues relating to party wall agreements are private matters to be resolved between property owners and not something that the Council can become involved with.

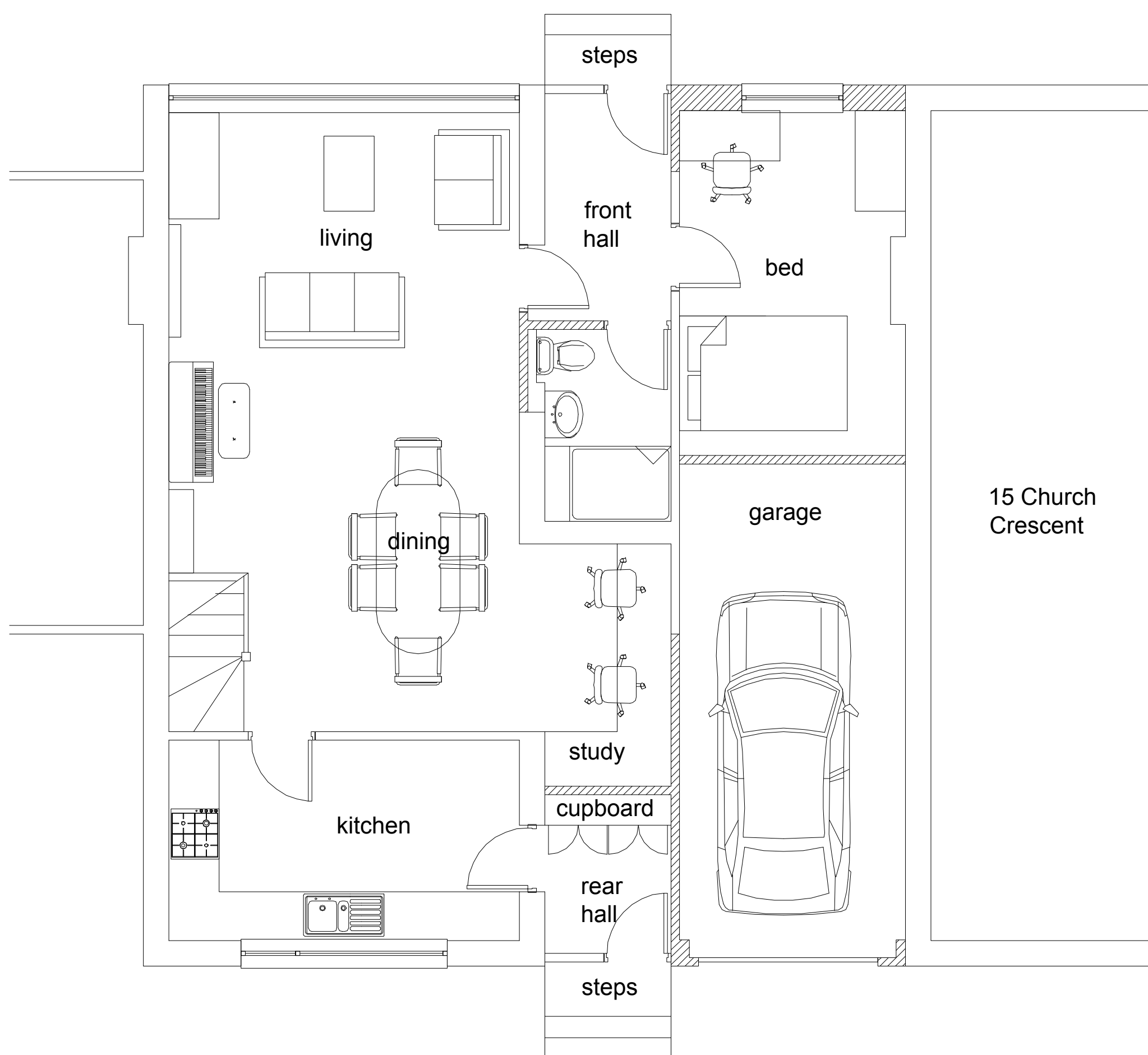
11.0 CONCLUSION

- 11.1 It is considered that, due to its location, size and design, the proposal would not cause harm to visual or residential amenity, is of acceptable design, and complies with the development plan and national and other local planning policy, including the Householder design Guide. There are no other material planning considerations that indicate planning permission should not be granted. Approval of the application is therefore recommended.

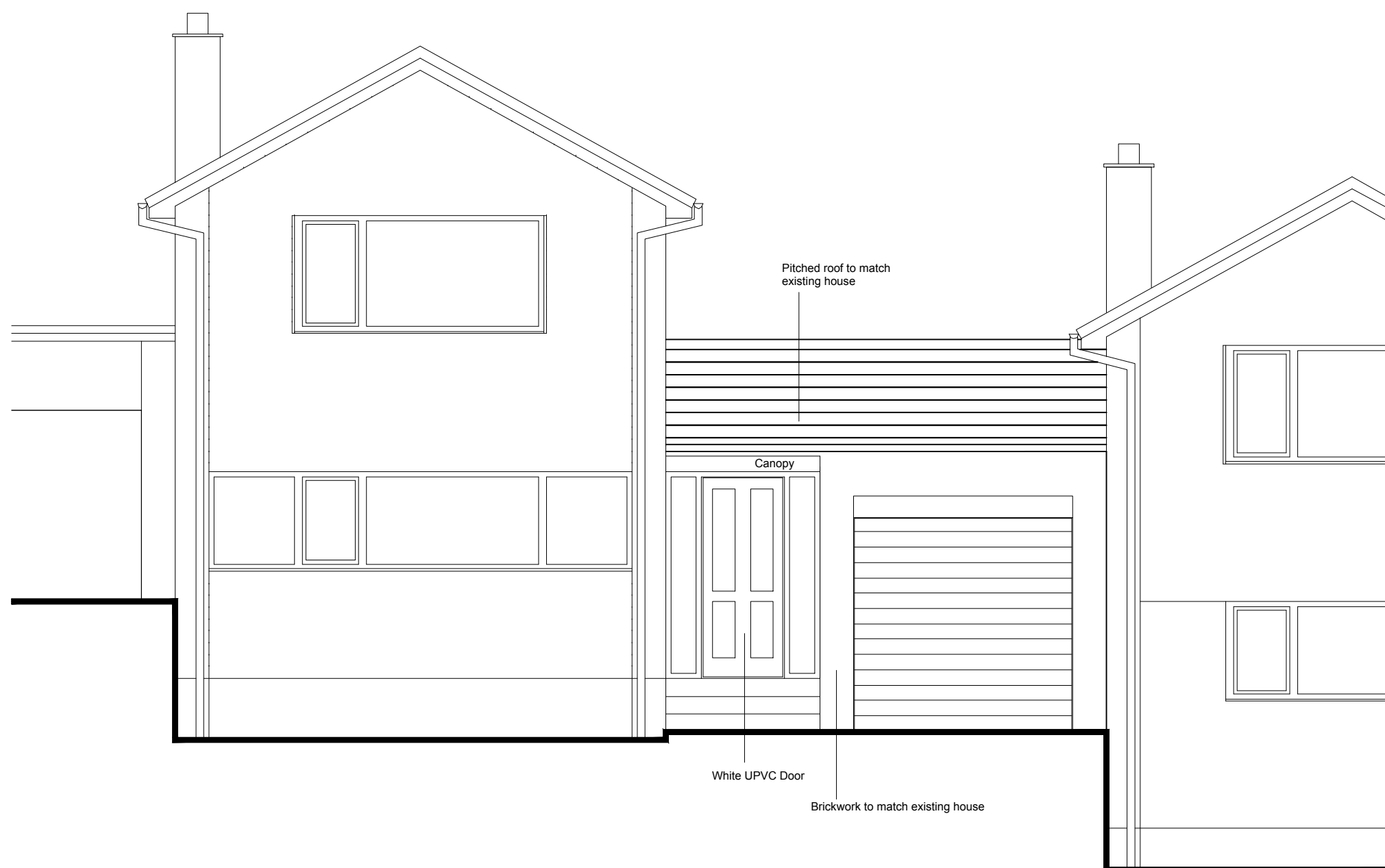
Background Papers:

Application files: 16/07825/FU & 16/00869/FU

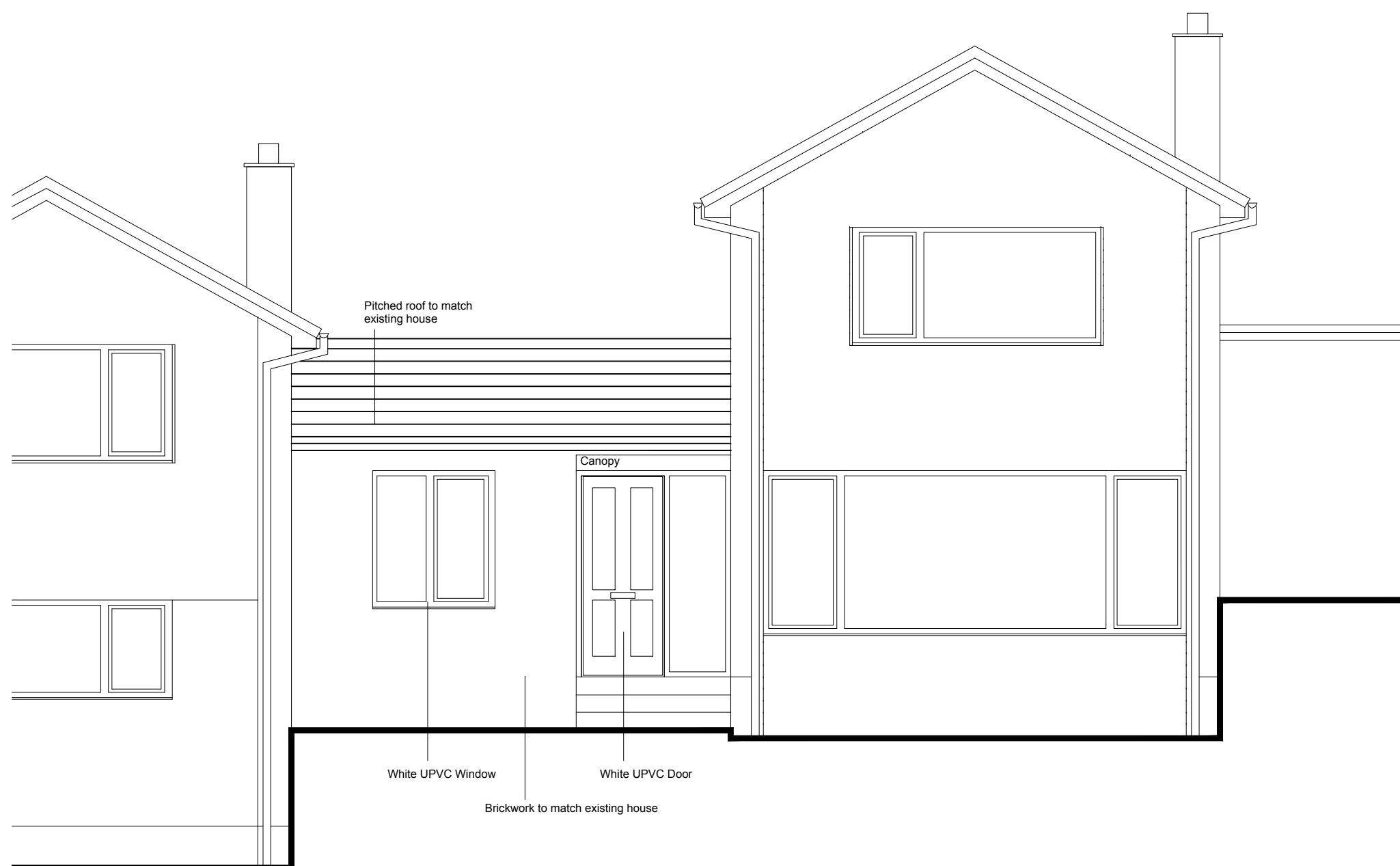
Certificate of ownership: Notice (Certificate B) has been served on the owners of 15 Church Crescent.



PROPOSED
GROUND FLOOR PLAN



FRONT ELEVATION



REAR ELEVATION

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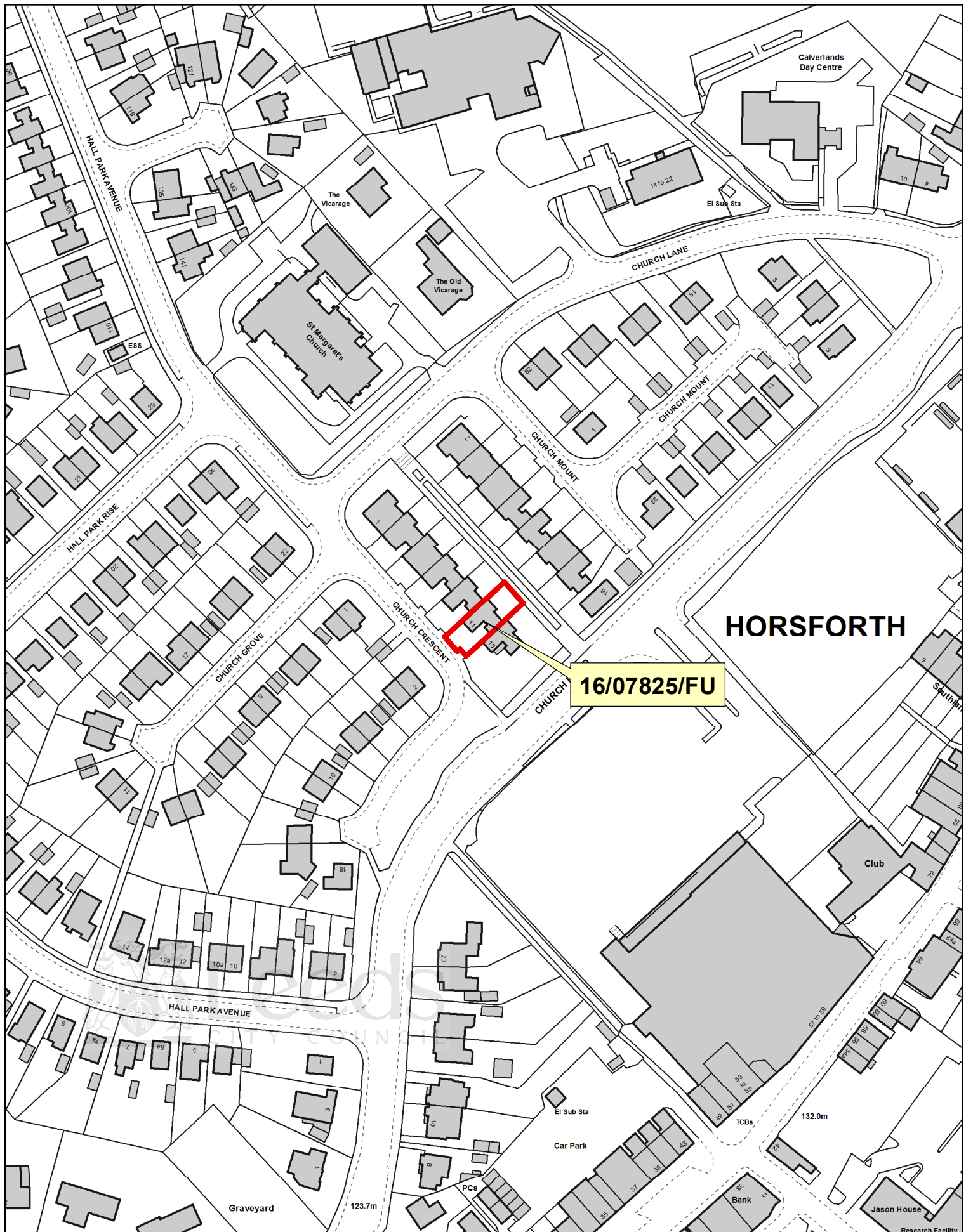
SCALE

Revisions	Date	Technician
A - RIDGE LINE AMENDED TO UNDERSIDE OF GUTTER LINE	22-01-17	M.V.E.

DO NOT SCALE OFF THIS DRAWING
Address 11 CHURCH CRESCENT HORSFORTH LEEDS WEST YORKSHIRE LS18 5LF

PROPOSED PLAN & ELEVATIONS
 Melissa Evans, 32 Woodlands Crescent, Gomersal, BD19 4SP Mob: 07989 174907 Email: evansmelsdesign@yahoo.co.uk

SCALE	A1 @ 1:50
DATE	DECEMBER 2016
DRAWN BY	M.EVANS
DRAWING NO	1055 - 03A



SOUTH AND WEST PLANS PANEL

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